

An architectural rendering of a shopping centre development. The scene shows a paved pedestrian area in the foreground with a person walking. To the left is a building with a 'SIGN' sign. In the background, there's a parking lot with cars and a 'Woolworths' sign. A large tree is on the right, and a brick planter box is in the middle ground. The sky is blue with clouds.

The Design Partnership

CRIME PREVENTION THROUGH
ENVIRONMENTAL DESIGN (CPTED) REPORT

KINGWAY PLAZA WOOLWORTHS SHOPPING CENTRE DEVELOPMENT

174-178 Lang Street,
Kurri Kurri NSW

September 2023


VOTRAINT NO124 PTY LTD

**CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED) REPORT
KINGSWAY PLAZA WOOLWORTHS SHOPPING CENTRE, SPECIALISED RETAIL AND PARKING.**

174-178 Lang Street, Kurri Kurri

PROJECT NUMBER: 22.018

PREPARED BY



Kristy Cianci (Ryan) | Managing Director, Architect & CPTED consultant.

Quality Control Plan

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A	1.09.23	DRAFT CPTED Report - not for lodgement with Council	KC	KC
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Report prepared by:

THE DESIGN PARTNERSHIP
TDP2 Pty Ltd ATF The Design Unit Trust T/A The Design Partnership
Central Coast – PO Box 6110 Long Jetty NSW 2261
T 02 4324 8554
E info@thedesignpartnership.com.au
W www.thedesignpartnership.com.au

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1 INTRODUCTION

The Design Partnership has been engaged by Votraint No124 Pty Ltd to prepare a Crime Prevention Through Environmental Design (CPTED) Report for the redevelopment of an existing shopping centre at 174-178 Lang Street, Kurri Kurri in New South Wales.

This report provides an assessment, analysis and recommendations for the design produced for the development.

1.1 What Is CPTED?

Crime Prevention Through Environmental Design (CPTED) is a strategy that aims to reduce crime by designing the built environment according to a set of guidelines.

CPTED is based on the principle that many offenders are guided by rational thought and make a cost/benefit analysis of their actions prior to committing a crime. Applying CPTED methods aims to discourage offenders by maximising the risk and effort of committing a crime, while minimising the benefits and opportunities of committing that crime.

CPTED also identifies ways to create a feeling of safety, leading to increased use of an area, which in turn improves natural surveillance and deters offenders.

In NSW, CPTED is largely administered by Safer By Design, a co-operative made up of NSW Police, local councils, government departments and private sector organisations. The Design Partnership structures its CPTED reports according to Safer By Design guidelines.

Safer By Design identifies seven key areas where CPTED principles can be applied: surveillance; lighting/technical supervision; territorial reinforcement; environmental maintenance; activity and space management; access control; design/definition/designation.

1.2 Scope & Methodology

The scope of this project is an assessment of the proposed redevelopment of an existing shopping centre at 174 - 178 Lang Street, Kurri Kurri (The Development). This CPTED Report forms part of a Development Application.

The following Methodology was used to prepare the CPTED report:

1. Desktop review of the design.
2. Review of crime data available through the Bureau of Crime Statistics & Research (BOCSAR).
3. Preparation of a letter to accompany the Development Application as requested by the Client December 2022.
4. Request to prepare this CPTED report and review revised documentation.
5. Site visit 23th August 2023.
6. Review of media and similar publications.
7. Discussions with the project team to discuss CPTED design considerations. Liaison with the Social Impact consultant from Mara Consulting.
8. Preparation of draft CPTED Report.
9. Preparation of final Report for lodgement with Council with the Development Application.

1.3 Structure Of The Report

The basis of this report is a Crime Risk Assessment (CRA), which is used to identify overall crime risk for the project and the appropriate level of CPTED treatments. A summary of the Crime Risk Assessment has been provided as diagrams in Section 4.

The basis of this report is a Crime Risk Assessment (CRA), which is used to identify overall crime risk for the project and the appropriate level of CPTED treatments. A summary of the Crime Risk Assessment has been provided as diagrams in Section 4. The full CRA assessment is provided in Appendix B. Using the CRA as a template, this report assesses the design and provides recommendations under the seven CPTED design principles.

1.4 About the Author

Kristy Cianci (Ryan) of The Design Partnership authored this report, who undertook CPTED training with the NSW Police in 2018. A copy of the certificate can be provided upon request.

Kristy is a registered Architect (ARB NSW No. 9254) who specialises in preparing CPTED assessments, studies, and reports and provides an expert witness role. Kristy also specialises in public art curation, strategy and policy development with a particular focus on the relationship between safety and public art.

1.4.1 Expertise

Urban Design Review Panels - CPTED expert

Kristy sits on three Urban Design Review Panels for Newcastle City Council, Lake Macquarie Council, and Port Stephens Council as an Architect and Urban designer - For each Panel, Kristy provides CPTED and public art advice.

Expert Witness for CPTED

Kristy currently acts as expert witness for CPTED on behalf of local government.

Place based CPTED Studies

Kristy undertook the CPTED study for the renewal of the East End of Newcastle. Kristy undertook CPTED assessments for Picton and The Entrance as part of the Place Plans.

Shopping Centre CPTED assessments and reports

Kristy has undertaken CPTED assessments for shopping centres and retail centres in NSW and Western Australia including:

- Chisholm shopping centre, which includes supermarket, speciality retail, childcare centre, 24 hour gym, medical centre, tavern and associated services and parking.
- Calderwood Village Centre which includes supermarket, speciality retail, childcare centre, 24 hour gym, medical centre, tavern and associated services and parking.
- Gledswood Village Centre which includes supermarket, speciality retail, childcare centre, 24 hour gym and pool, food and beverage premises, medical centre, movie theatre, residential and associated services and parking.

- The Saleyard, Midlands Western Australia comprising big box retail, 24 hour gym, childcare centre, petrol station and associated services and parking.
- Queens Street, Campbelltown mixed use development comprising supermarket, retail, commercial, food & beverage, residential, child care, gym, associated services and parking, neighbourhood centre and park.

Experience in other CPTED project typologies includes boarding houses, mixed use developments, residential flat buildings, research laboratories and manufacturing facilities, artist residency / performing arts centre, aged care and assisted living and sporting precincts. Further information about these projects can be provided upon request.

1.5 Council's CPTED Guidelines

Council has a statutory obligation under the provisions of Section 4.15 of the Environmental Planning and Assessment Act 1979 to consider:

- The likely impacts of a proposed development (submitted via a development application), including social impacts in the locality, and
- The public interest

CPTED provides an assessment of a proposed development and whether it improves safety or increases it. Safety can be both actual and perceived.

Cessnock City Council's Development Control Plan (DCP) has a chapter titled "Guidelines: Crime Prevention Through Environmental Design". These guidelines are provided by Council to assist Development Applications within the LGA. Table 1 of the guidelines identifies the types of development requiring a CPTED Assessment Report. This development fits under the category "New commercial and specialised retail premises with an estimated cost of development of \$5 million or more", thereby requiring the preparation of this report. The Guidelines identify the minimum standards for a CPTED Assessment Report and an overview of CPTED principles.

This report is also guided by the guidelines, along with the training and documentation provided during the training with NSW Police.

2 THE WIDER CONTEXT

2.1 The Context of Kurri Kurri

Kurri Kurri is located in the Hunter Region, within the Cessnock Council Local Government Area (LGA), 40 kilometres west of Newcastle.

The City of Cessnock covers 1,966 square kilometres and is known as Australia's oldest wine producing region. Its vines and wine producers fuel a wine based tourism economy. Cessnock, located to the west of Kurri Kurri is the administrative centre of the LGA.

Kurri Kurri is one of the largest towns in the LGA. The town is highly accessible by car, only a short distance from the Hunter Expressway, which has an on and off ramp that connects to the town's main street. Lang Street, the town's main street is wide, with a central parking aisle and parking along its street edges. The centre is a mix of single storey and

shop top housing. The ground level is activated by various businesses that serve the local community.

The town is known for its murals, of which it has over 60. Each mural tells the town's story and features a kookaburra, the town's mascot. Most are attributed to the artist and explain the story or theme of the mural. During the site visit, visual inspection observed high levels of graffiti and tagging on buildings, walls, fences, and utilities.



Figure 1: Kurri Kurri, west of Newcastle CBD and east of Cessnock (ArcGIS 2023 /TDP 2023)

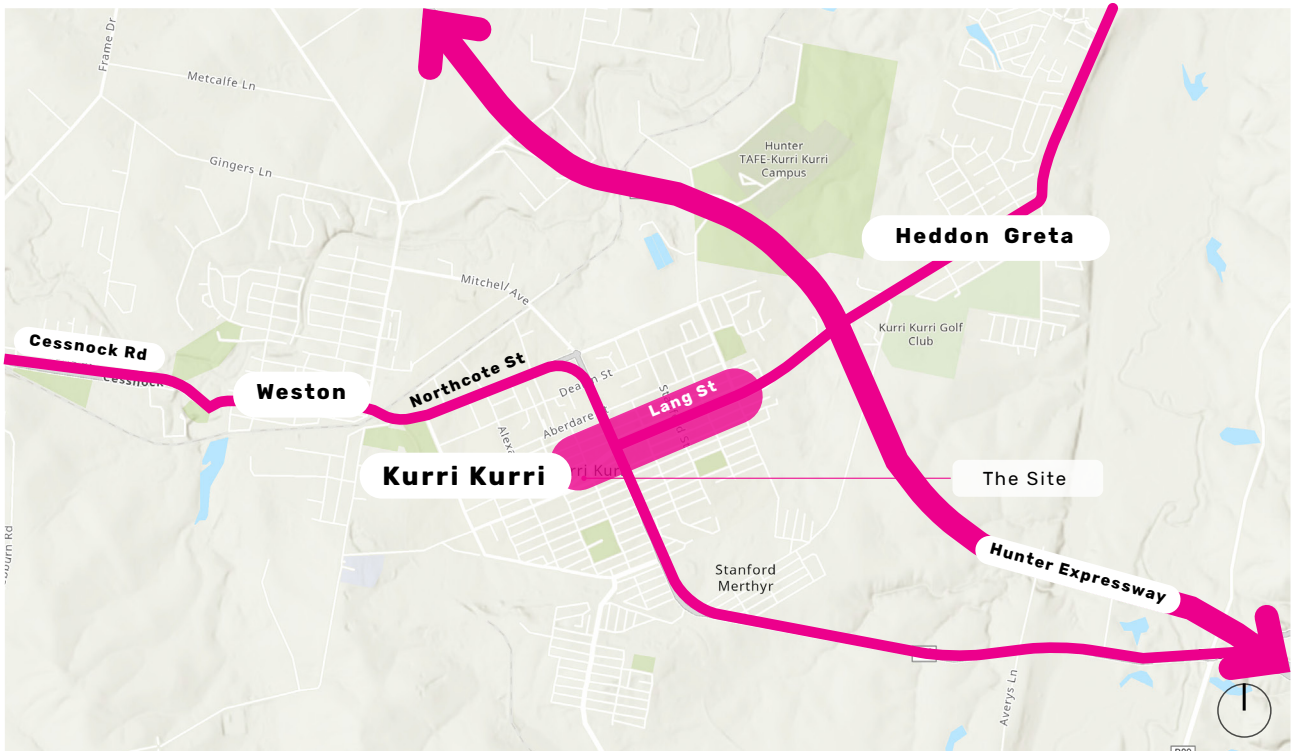


Figure 2: The subject site in relation to neighbouring Weston and Heddon Greta (ArcGIS 2023/ TDP 2023)
 The site is located west of the Hunter Expressway and south of Lang Street, the main street of the town.

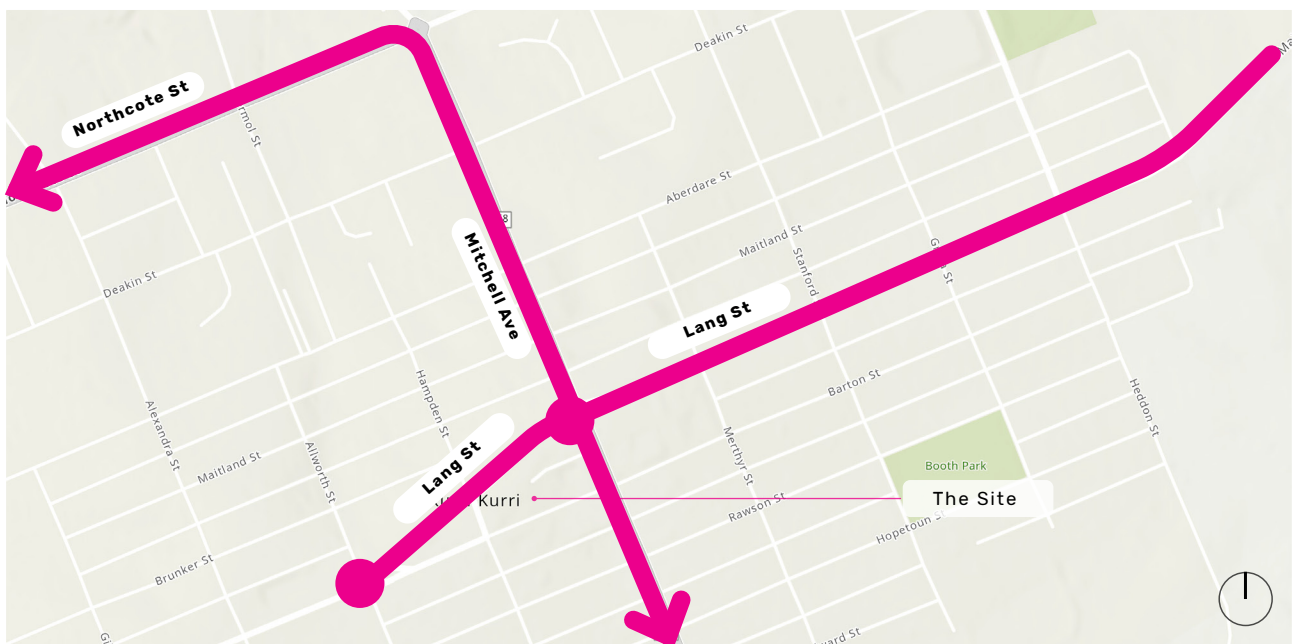


Figure 3: The subject site within Kurri Kurri (ArcGIS 2023/ TDP 2023)
 The site is located south of Lang Street, the main street of Kurri Kurri.

2.2 Social Analysis of Kurri Kurri

Kurri Kurri is located within the Cessnock City Council Local Government Area. Data from Profile.Id.com.au shows Cessnock City has a population of 65,780 residents (2022 ABS Estimated Residential Population (ERP)) in a land area of 1,966 square km with a density of 33.46 people per square km.

The subject site is located within the Kurri Kurri - Pelaw Main catchment. Refer to Figure 5.

The Kurri Kurri - Pelaw Main catchment has a population of 7,965 residents (ERP) in a land area of 15.20 square km with a density of 524 people per square km.

Household Types and Size

The largest household type is the Lone Person households at 29.4% (LGA 23.2%) followed by Couples with Children at 23.1% (LGA 28%) and Couples without Children at 22.5% (25.4% LGA). In Kurri Kurri, the households are generally smaller with 33.5% comprising only two people, and 30.71% comprising one person.

Dwelling Types

Separate houses are the largest dwelling typology at 84.1% compared to 89.8% in the LGA. Medium Density living is at 15.6% which is above the LGA rate of 9.6%.

Car usage

Car ownership is predominately one or two cars. 69.3% of households have one or two cars. 6.2% do not have a car which is above the LGA which is 4.3%. The no car cohort may rely on walking, public transport or getting a lift which can improve activation of the street. At present, the majority of residents travel by car to work (69.4%) while only 0.4% use public transport. These statistics are not unusual as 55.1% employed residents work outside of the LGA and the public transport network is limited.

There was a notable increase in working from home during the last census; however, the 2021 census was held during a COVID lockdown, which required many people to work from home. However, the preference for working from home has continued for many workers and may continue to be higher than the previous census. Working from home is beneficial as it increases the use of local businesses and activation and natural surveillance of its streets.

Birthplace and language

0.4% of residents were born overseas (7.2% LGA). The highest proportion of overseas residents are from United Kingdom, New Zealand and the Philippines.

The ability to speak English (as well as another language) ranks high at 92.6% (90.2% LGA). The number of residents who do not speak English well or at all is low at 0.4%. This is an important factor when signage is relied upon for CPTED solutions. Signage considerations will also be important to future proof the development should demographics change in the future, particularly as Kurri Kurri is located within a tourism destination.

Vulnerability

42.8% of residents identified as having at least one long term health condition. This is above the LGA which is 39%. The catchment also has a high proportion of people over the age of 60, which has grown since the last census. Good CPTED design considers the vulnerability of its potential patrons who could be at risk of theft or assault.

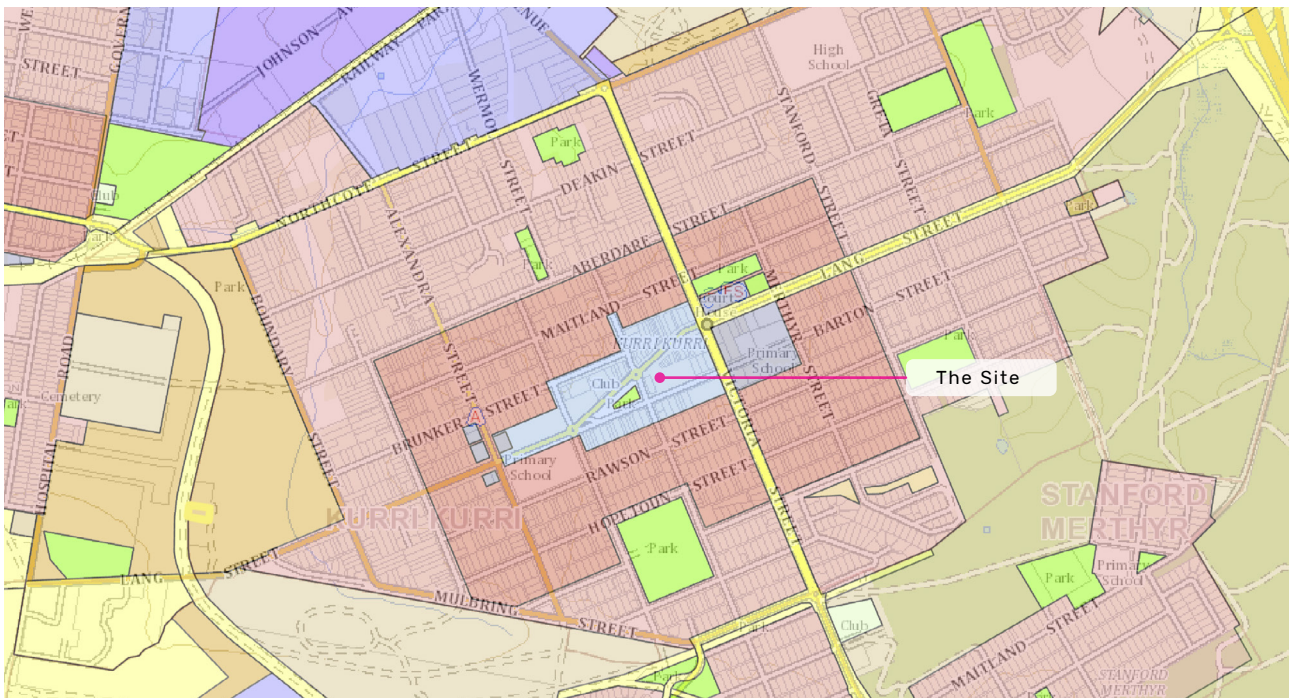


Figure 4: Kurri Kurri Land Zoning Map (Spatialviewer 2023)

The site is at the heart of the E1 Local Centre zoning (blue). The Local Centre is ringed by R3 Medium Density. Bo, Brown Rotary Park is zoned RE1 Public Recreation and is located to the west of the site.

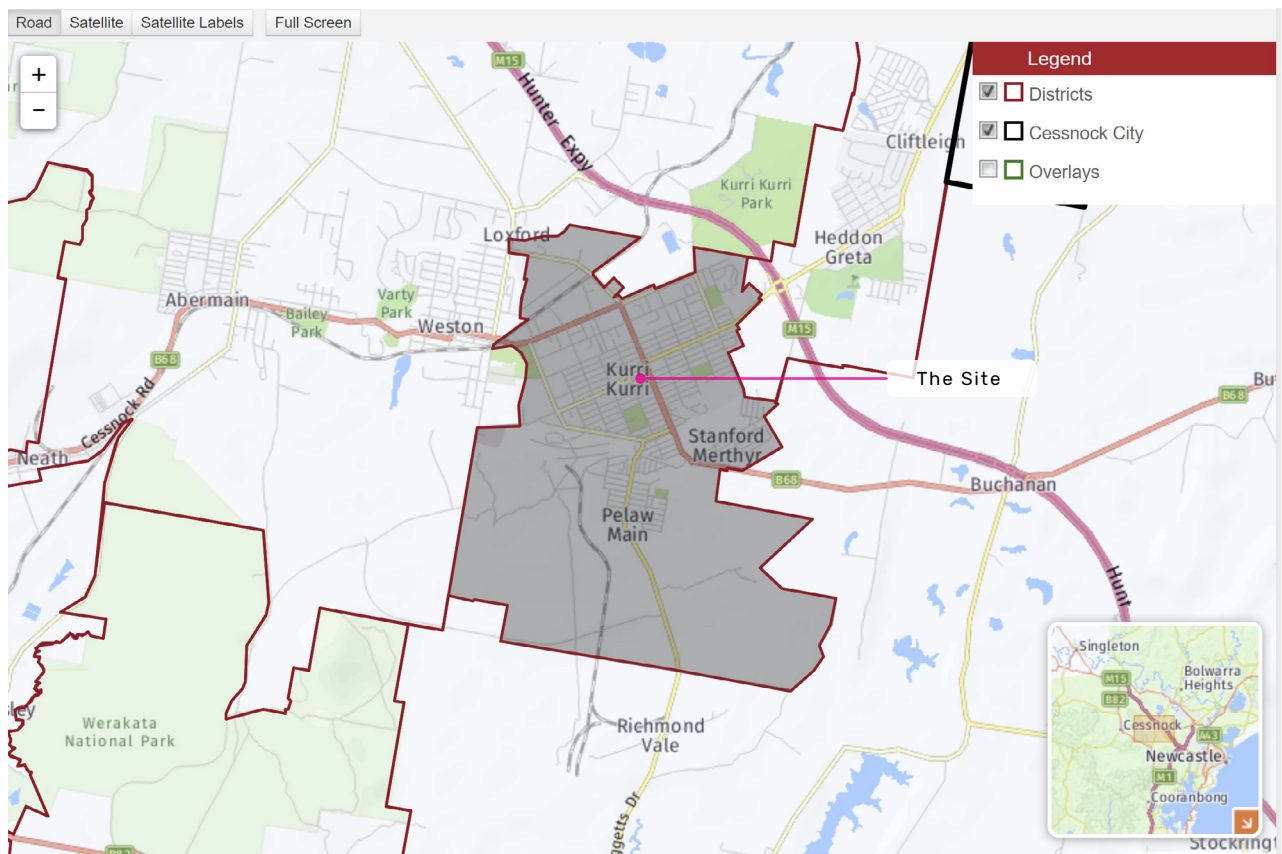


Figure 5: Profile Id location map

Kurri Kurri - Pelaw Main catchment within the Cessnock City Council LGA (ProfileID 2023)

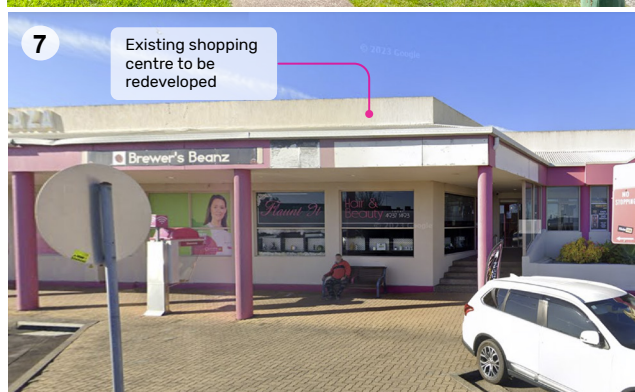


Figure 6: The local context photos of the site and surrounds (TDP 2023)

1. Shopping centre entry from carpark.
2. Looking east along existing laneway towards Victoria Street.
3. Kurri Kurri Hotel viewed from Hampden Street.
4. Kurri Kurri Hotel back of house which will be exposed post construction.

5. Existing shopping centre fronting Barton Street.
6. Rear of rear fronting laneway. Drive thru at head of driveway.
7. Lang Street entry to shopping centre.
8. Existing laneway looking west towards shopping centre.

3 THE PROPOSED DEVELOPMENT

The proposed development will be on the site known as the Kingsway Plaza shopping centre. The shopping centre has frontage to Lang Street to the north, Barton Street to the south and the Kurri Kurri Hotel to the west. An unnamed laneway traverses through the open air car park to the east.

The development proposes a new Woolworths supermarket including associated offices, storage and plant, three (3) new speciality shops, a new plaza, carparking and loading areas.

A summary of the development is provided below and in further detail adjacent.

Table 1: Development Summary
Woolworths Supermarket Woolworths supermarket located at Lang Street level and above basement carpark. It will comprise: <ul style="list-style-type: none"> Woolworths – 3,375sqm. Mezzanine comprising plant and offices – 409sqm. Loading area – 173sqm. Trading Hours – 6am to 12 midnight, 7 days a week
Speciality Retail Three (3) speciality retail spaces comprising 650sqm. <ul style="list-style-type: none"> Speciality Retail T1 – fronting open air carpark Speciality Retail T2 – fronting Barton Street Speciality Retail T3 – fronting Lang Street and new plaza. Trading Hours – 9am to 6pm, 7 days a week
Carparking Two carparks comprising: <ul style="list-style-type: none"> Open air carpark located at the Woolworths entry level providing 32 parking spaces. Basement carpark providing 114 parking spaces, plus 6 direct to boot parking spaces.
Plaza and Street <ul style="list-style-type: none"> New plaza provides connection between Lang Street and the supermarket and it provides a mid block connection to Barton Street. 25 parking spaces on Barton Street.

Woolworths Supermarket

The Woolworths supermarket will be located at the Lang Street level and its pedestrian entry will be from a small forecourt and an open air carpark which is accessible from the existing laneway.

A mezzanine area comprising offices and plant is located at the eastern end of the supermarket and is accessible via stairs.

Vehicular and pedestrian access to the basement carpark below is available via a ramp and stair, located on the western side of the open air carpark.

Woolworths will trade 6am to 12 midnight 7 days a week.

Specialised Retail

Three (3) specialised retail premises are proposed. T1 is located at the entry to Woolworths. T2 is located on Barton Street, which assists with activation of the street. T3 is located between Lang Street and the laneway and addresses the new plaza. T3 is likely to be a cafe which provides good surveillance.

The retail will trade from 9am to 6pm, 7 days a week.

Open Air Carpark and Basement Carpark

There are two parking areas proposed. The first is an open air carpark and the second a basement carpark.

The open air carpark will provide 32 parking spaces and will be accessible from the existing laneway. Two accessible parking spaces are located at the entry to Woolworths. A vehicular ramp and stair provides access to the basement below. Planters are proposed in the parking area.

A large basement carpark is proposed under the Woolworths building and will provide 120 parking spaces. Direct to Boot deliveries for Woolworths patrons are proposed in the carpark. Pedestrian access to Woolworths above is via a traveller, and

the lift. Both take pedestrians to the forecourt above. Stairs are also proposed that provide access to the open air carpark above. Amenities are proposed in the basement carpark along with the T2 specialised retail.

The basement carpark will be open to align with Woolworths hours of operation which is 6am to 12 midnight.

Lift

A lift is proposed that is accessed from Barton Street and takes pedestrians to the Woolworths forecourt above. The lift has its own glazed lobby at the Barton Street level which has doors that separate it from the basement carpark.

Council proposes the lift to be open to the public 24/7, 7 days a week.

Loading Area

A loading area is proposed under the Woolworths supermarket and adjacent the basement carpark. The loading dock is for both Woolworths supermarket and the Speciality Retail.

The loading dock will be open 5am to 12am.

Plaza

The demolition of the existing shopping centre provides the opportunity for a new public plaza. The small plaza will provide a pedestrian connection between Lang Street and the new shopping centre. It will also improve the permeability of the block, which currently has only one other laneway; heavily graffiti and tagged.

The plaza will be public and open 24/7, 7 days a week.



Figure 7 Artist impression of the development as viewed from Lang Street.

New specialised retail (T3) fronting Lang Street which is likely to be a cafe. The new plaza is between T3 and the Kurri Kurri Hotel. The proposed Woolworths Shopping Centre is behind.

4 CRIME RISK ASSESSMENT SUMMARY

4.1 Crime Prevention Assessment Site & Building Analysis

Site analysis helps in the understanding of a place – how the built form, landform and local user groups can increase or decrease criminal activity.

The following diagrams are a visual representation of the Crime Risk Assessment (CRA) undertaken for this project. The outcomes of this analysis are incorporated into the Assessments and Recommendations provided in Sections 5 – 12.

4.1.1 Context Analysis

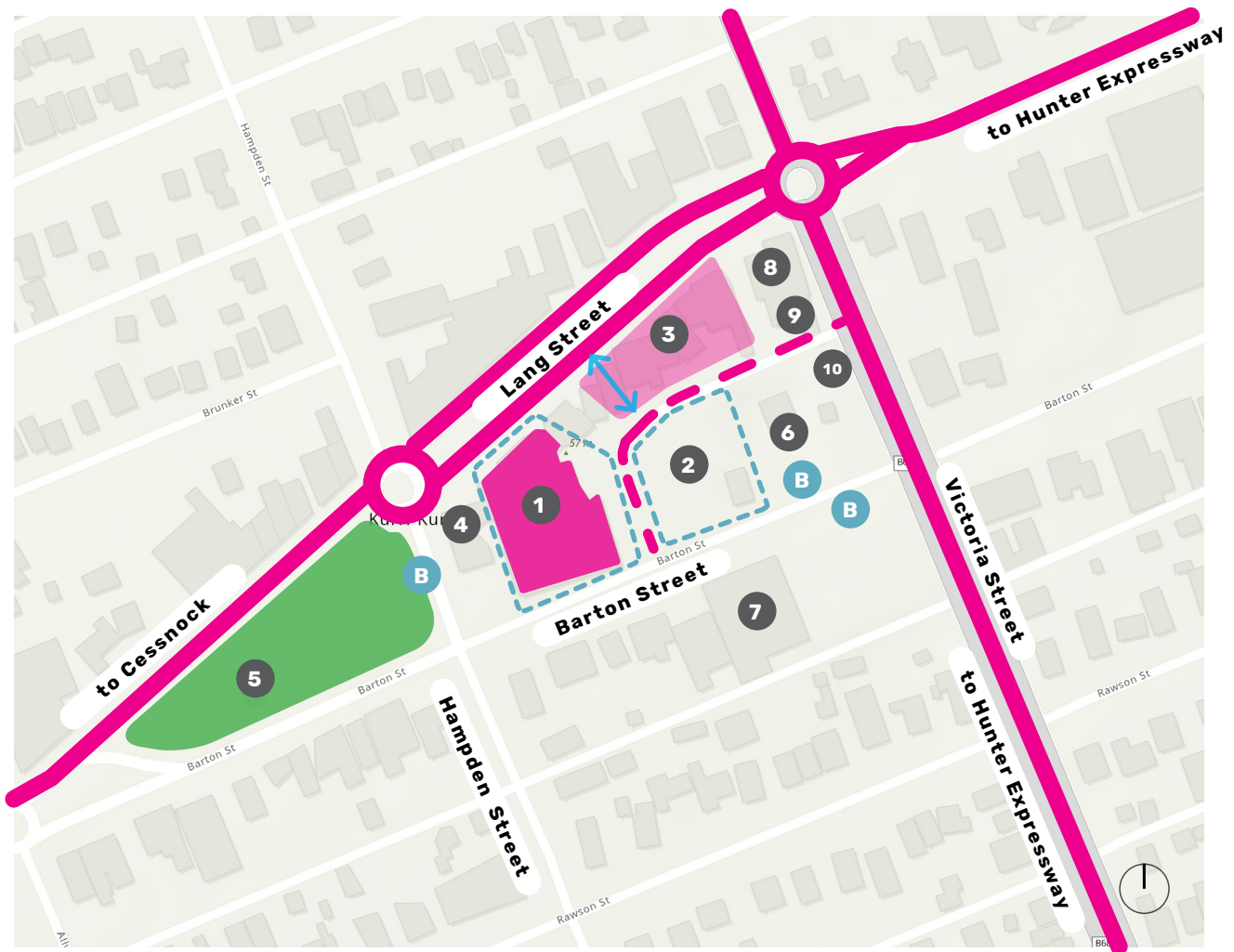
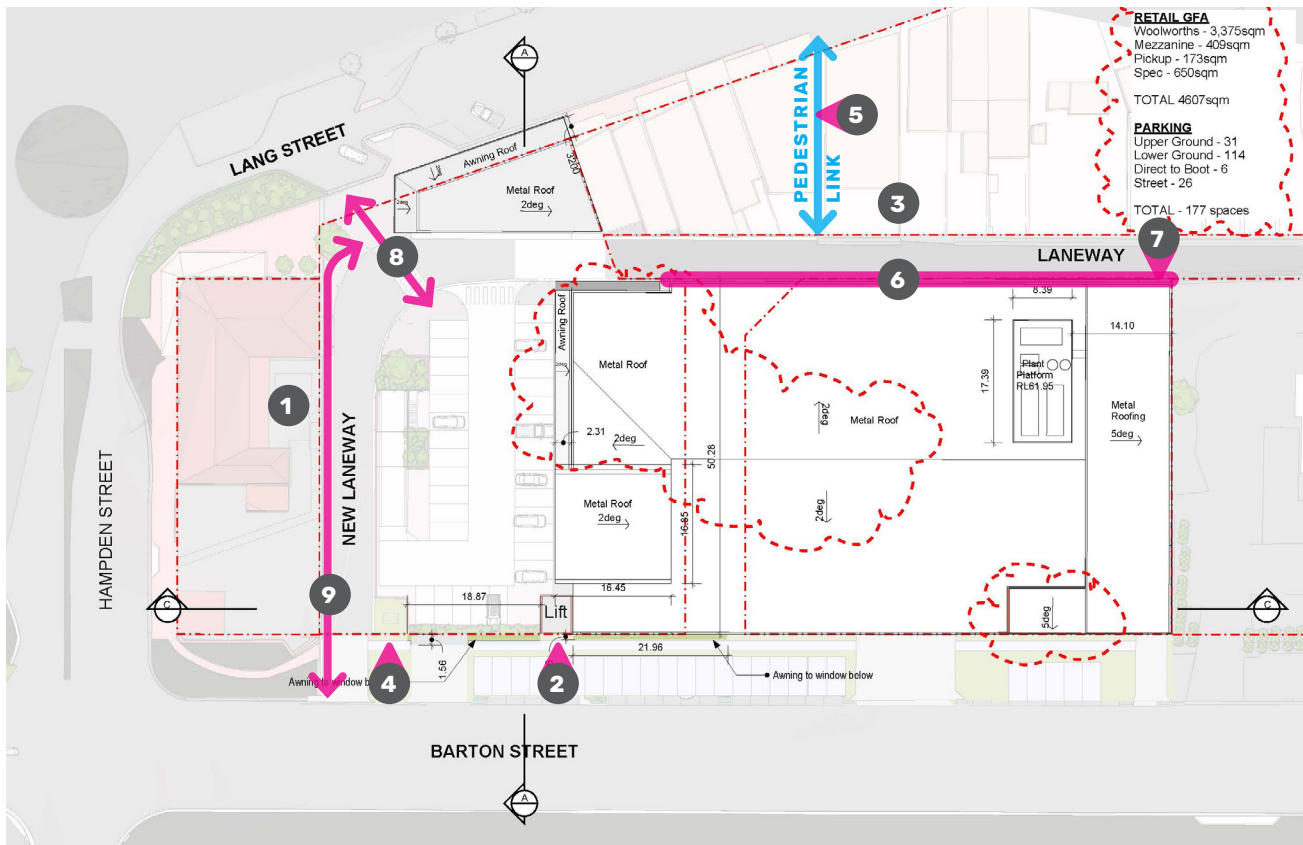


Figure 8: Summary Crime Prevention Assessment - Context Analysis (ArcGIS / TDP 2023).

The subject site is located south of Lang Street, the main street of Kurri Kurri. A shopping centre and carpark currently occupy the site. The shopping centre is ageing and is challenging to identify as a shopping centre. Its entries are small and setback (1). The existing open air carpark is used by patrons of the shopping centre and surrounding area. It is perceived as a public carpark but is private for the shopping centre (2). It generally has good sight lines, except for the screening trees that line Barton Street and the laneway. The laneway currently services the retail fronting Lang Street. Their rear setbacks are generally open and unfenced and allow access to the rear doors of the premises (3). Kurri Kurri Hotel (4) is located to the west and shares a boundary with the existing shopping centre. Following development, the eastern boundary of the hotel will be open and accessible. While not fully investigated, this area may need improvements to security. Col Brown Rotary Park (5) is located west of the site. It was undergoing renovation, and the remainder of the site was well cared for. The park includes public amenities. Chelmsford Hotel (8) is located east of the site and has a drive-thru bottle shop accessible from the laneway. Bus stops close to the site improve accessibility to the town centre. Malicious damage in the form of illegal graffiti and tagging was observed in the town.

- | | | | |
|--|-------------------------------------------------------|--|-------------------------|
| | Subject site. | | Col Brown Rotary Park |
| | Primary movement. | | Bus stop |
| | Existing laneway and extension to Barton Street. | | |
| | Pedestrian lane connects to Lang Street. | | |
| | Existing Shopping Centre | | Existing KFC |
| | Existing open air carpark | | Existing Aldi & carpark |
| | Retail fronting Lang St which backs onto the laneway. | | Chelmsford Hotel |
| | Kurri Kurri Hotel | | Drive thru bottle shop |
| | Col Brown Park/Public Amenities | | Scout hall |

4.1.2 Site Analysis

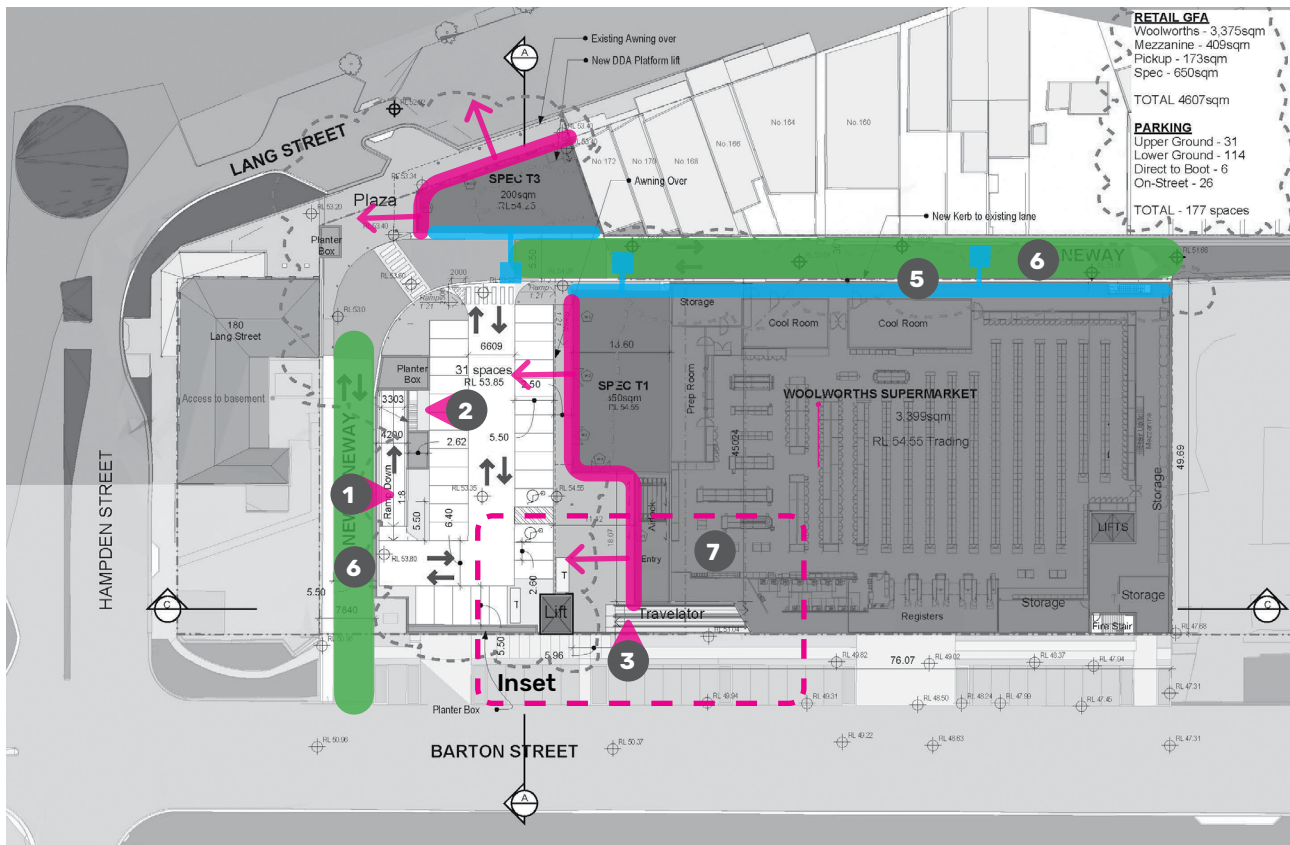


- 1** Once the existing supermarket is demolished the eastern side of the hotel is exposed. A new timber fence is proposed to secure the hotel.
- 2** Lift is recommended by Council to be open 24/7 to provide an accessible mid-block connection. The lift could be subject to anti social behaviour, malicious damage and public urination. By remaining open it also creates a concealment and entrapment alcove in the street which could be used by offenders. Further investigation is recommended to determine who will use the lift after the centre closes at midnight.
- 3** Existing retail does not provide good surveillance of the laneway. Many back of house areas could benefit from organisation and maintenance. Once the laneway is improved, Council are encouraged to support activation programs in the laneway.
- 4** The substation has a concealment and entrapment space behind which will need to be managed with landscape treatment.
- 5** The walls of the existing pedestrian laneway are covered with illegal graffiti. It is likely that the new development will experience malicious damage and should be designed to mitigate it. As the whole precinct is experiencing malicious damage, Council are encouraged to consider supporting local businesses in a rapid removal cleaning processes and a new mural in this pedestrian lane to reduce the likelihood of future graffiti.
- 6** Wall at risk of future illegal graffiti due to poor surveillance and large surface area.
- 7** Alcove from fire stair is a concealment and entrapment spaces which will need to be managed with CPTED approaches.
- 8** New plaza connection will improve access to the shopping centre and Barton Street. However, it may also open up this precinct to new crime activities which are presently reduced by the building.
- 9** Use of the new footpath will provide natural surveillance for the precinct.

Figure 9: Summary Crime Prevention Assessment - Proposed Site Plan (Ben Pomroy 2023).

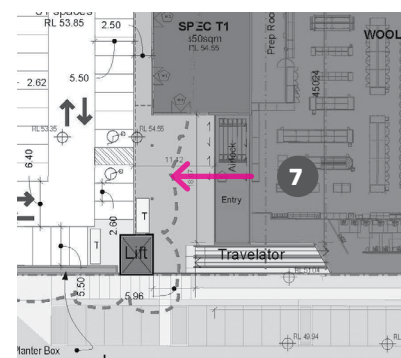
The Site Plan shows the future planned development for the subject site. The existing laneway is proposed to be formally extended to connect with Barton Street. This will include a new footpath that will connect pedestrians to Lang Street via a new plaza. The new plaza allows the supermarket to form part of the main street activities which assists with activation.

4.1.3 Building Analysis - Specialised Retail and Basement Carpark



Lang Street / Laneway Level

- 1 The ramp to the basement is not secure and allows both vehicles and pedestrians to access after hours. This leaves the basement carpark
- 2 Door to the basement is at the bottom of the stairs. This creates an entrapment and concealment space and its walls are likely to experience malicious damage.
- 3 The traveller is not secure which leaves the basement unsecured after hours.
- 4 The lift exits into the Woolworths forecourt space.
- 5 The laneway walls are likely to experience malicious damage (illegal graffiti). There are large areas affected by graffiti on site. Murals and/or materials make graffiti hard to see and can discourage it.
- 6 There are also low levels of surveillance in the laneway due to level changes and as surrounding development turns its back to the lane, which increases risk.
- 7 Surveillance from supermarket over carpark however, is reduced due to extent of T1 retail when compared to other supermarkets.

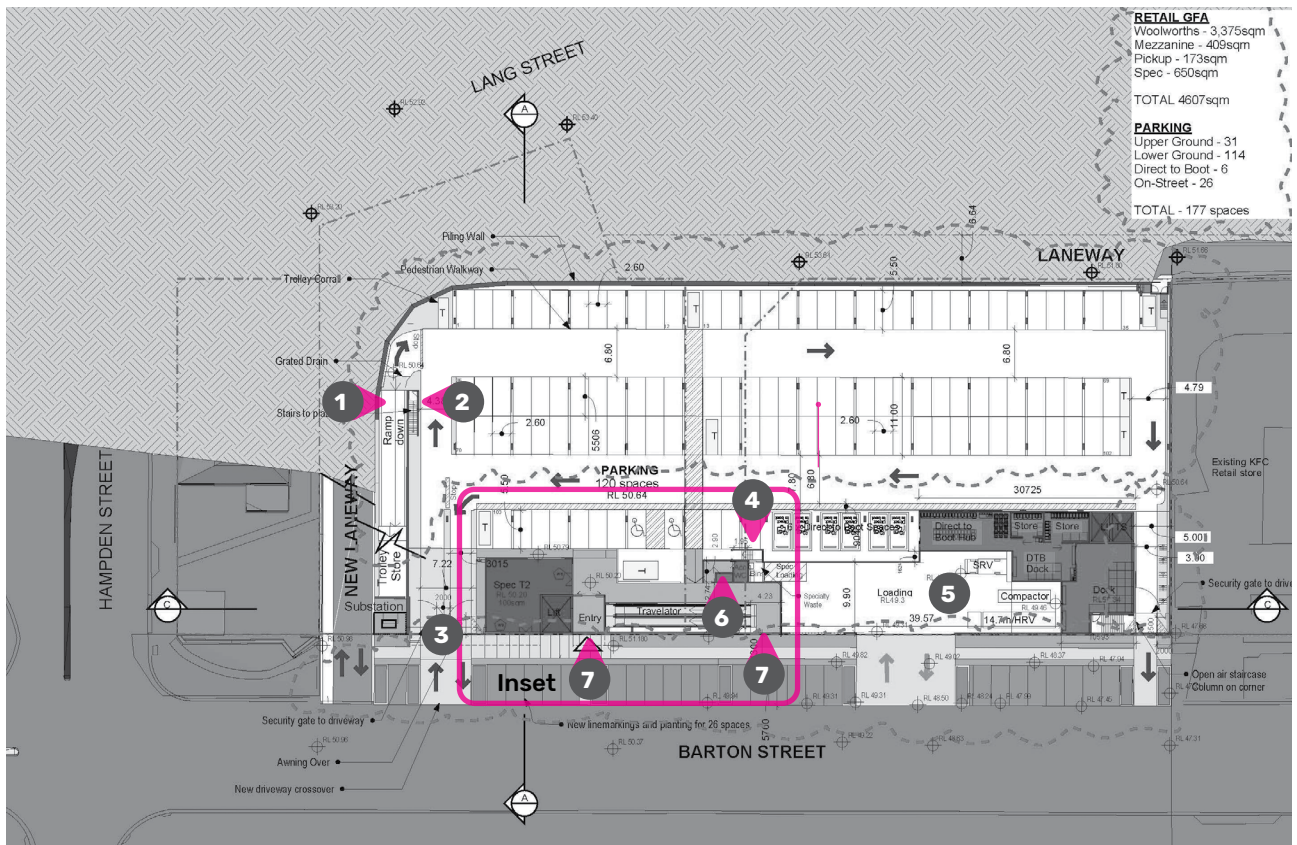


Inset - Lift & Travellator

Figure 10: Summary Crime Prevention Assessment - Proposed Woolworth Level (Ben Pomroy/ TDP 2023).

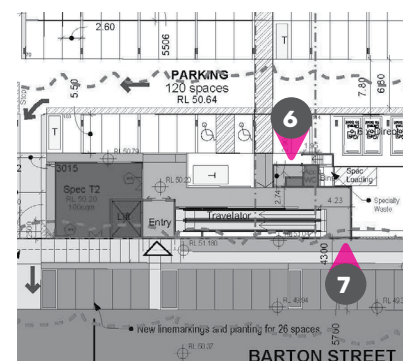
(top) The Woolworths supermarket is located at the Lang Street level. An open air carpark is located to the west of the Woolworths entry and is accessed from the laneway. The laneway has low levels of surveillance due to the orientation of the Lang Street retail and the Kurri Kurri Hotel and topography changes. The new shopping centre will not improve natural surveillance of the laneway however, is likely to increase the number of people in the laneway to access the new shopping centre. The formation of the new plaza should also increase pedestrian movement through the site which can improve natural surveillance.

4.1.4 Building Analysis - Basement Carpark



Barton Street / Basement Carpark

- 1 The ramp to the basement is not secure and allows both vehicles and pedestrians to access after hours.
- 2 Door to the basement is at the bottom of the stairs. This creates an entrapment and concealment space and its walls are likely to experience malicious damage.
- 3 A secure garage door is proposed from Barton Street. This door will be closed when the centre closes.
- 4 The Loading area is a semi-private space and should be secured from the basement carpark. A door will be needed.
- 5 The loading area should remain secure when there are no deliveries.
- 6 The amenities are accessed via a corridor and are concealed. Improved access and sight lines can reduce risk of entrapment and malicious damage.
- 7 The travellator is not secure above which leaves the basement unsecure after hours. The exit from the travellator has reduced sight lines from the carpark as it has been blocked by the amenities
- 8 Council proposes the lift remain open 24/7, this results in an alcove in the street which is a concealment and entrapment space.



Inset - Lift, Travellator & Amenities

Figure 11: Summary Crime Prevention Assessment - Proposed Basement Carpark (Ben Pomroy/ TDP 2023).

The basement carpark is proposed to be open until midnight while the Woolworth supermarket trades above. Late trade leaves the carpark and its users vulnerable, noting that non-domestic assault, sexual assault, theft and malicious damage are well above the NSW average. Access Control, the reduction of concealment spaces and careful material selection will be important during design development.

4.2 Crime Prevention Assessment: Crime Data

Analysis of crime data has limitations that are important to consider when undertaking an assessment of a place and its proposed development.

Crime data available for the suburb will not specify if it relates specifically to the study area. However, hotspots can provide an indication if there is activity happening to a specific area. Hotspots are noted in Table 2 below and mapped in Figures 12 - 18.

It is also important to note additional limitations with regard to the reporting of crime, that 'the reporting of crime is influenced by a number of factors, including public understanding of what constitutes a crime and the public's willingness to report crime.' (Weatherburn 2011).

Data used for crime assessments are from NSW Bureau of Crime Statistics and Research (BOCSAR).

Crime Data Summary

The assessment of crime data was focused on activities that relate to the future operations.

For this development it includes, crimes relating to retail premises, carparks, the public domain (crimes that could occur on the street, lane or plaza).

Crime Data Snapshot	Year to March 2022		Year to March 2023		Hotspot
Offence	Kurri Kurri Rate per 100,000	NSW Rate per 100,000	Kurri Kurri Rate per 100,000	NSW Rate per 100,000	yes/no n.c not counted
Assault: Non-Domestic Assault	578 (36)	346.7	690.4 (43)	391.7	Yes - edge
Robbery	0 (0)	21.2	32.1 (2)	23.3	No
Sexual Offences	321.1 (20)	177.8	353.2 (22)	188.7	n.c
Theft	2922.3 (182)	2071.1	3676.9 (229)	2254.2	n.c
Theft: Break and Enter Non-Dwelling	96.3 (6)	89.6	433.5 (27)	93.1	Yes
Theft: Steal from Retail	353.2 (22)	217.3	433.5 (27)	299.1	n.c
Theft: Steal from Person	32.1 (2)	20.9	16.1 (1)	25.4	Yes (2021)
Theft: Motor Vehicle Theft	417.5 (26)	130.8	481.7 (30)	158.6	Yes (2022)
Theft: Steal from Motor Vehicle	385.4 (24)	339.3	802.8 (50)	338.4	Yes
Malicious Damage	1188.2 (74)	597.9	1236.4 (77)	604	Yes
Disorderly Conduct	176.6 (11)	211.6	256.9 (16)	232	n.c

Table 2 : Crime data snapshot

Data for rates per 100,00 over a two year period for Kurri Kurri - March 2022 and March 2023. The table shows that crime activities relating to non-domestic assault, robbery, sexual offences, vehicle theft, break and enter for non-dwellings, theft from retail and malicious damage. There are hotspots on the site for break and enter non-dwellings, steal from person, steal from vehicle and malicious damage. The site is on the edge of a hotspot for non-domestic violence which is generally consistent over a period of time. N.C refers to hotspots not provided in BOCSAR.

Summary of Findings.

Analysis of BOCSAR data for Kurri Kurri identifies a number of risks that should be considered in design development and future management of the shopping centre.

Assault

Rates for Non-Domestic Assault are above the NSW average for both 2022 and 2023. There is a consistent hotspot in Lang Street which overlaps the site periodically. In 2018 it was directly over the subject site.

Sexual Offences is also above the state average. Kurri Kurri's rates for Sexual Offences are above neighbouring Weston and Heddon Greta.

For both Non-Domestic Assault and Sexual Offences, the proposed basement carpark, the amenities and service areas of the building will need to be well considered to reduce or remove areas of concealment and entrapment as many areas used for Sexual Offences are less than 10m² and are enclosed by one or two boundaries.

Theft

Thefts relating to motor vehicles are well above the NSW average. These include the theft of motor vehicles themselves and belongings from motor vehicles. The site is located within a hotspot for Steal from Motor Vehicle. As there are two types of carparks, each has a slightly different but similar strategy.

Basement carparks need to provide good sight lines by reducing obstructions and providing good lighting. The basement carpark must be secure after hours to minimise the risk of assault and malicious damage. This is particularly important in the evening when the number of patrons has reduced, and the supermarket staff and other patrons will

provide less surveillance. The open air carpark will have better surveillance in the evening as windows overlook part of the carpark.

Theft from Retail Premises and Break and Enter Non-Dwelling demonstrates a risk for Woolworths and the specialised retail. The rates are above the state average, and there is a hotspot for Break and Enter Non-Dwelling over the site. The specialised retail (T2) on Barton Street is more isolated than the other two premises as they are located on the main street and at the entry to the supermarket. However, the retail unit may provide some surveillance for the basement carpark until it ceases trade at 6pm. The treatment of its glazing will be important to provide good sight lines which is discussed in Section 6.

Malicious Damage

Illegal graffiti and tagging were observed on site. The locations ranged from highly visible public walls through to concealed spaces. The town is well known for its murals and it was noted that none of the murals observed were defaced or marked with graffiti. Murals are used to discourage graffiti as if well designed, can make it difficult to identify the tag.

It's important to note that the existing crime data cannot predict the future and the dynamics of the future population. Therefore, designs should consider that new anti social and criminal activity could take place in this area in the future. This requires resilient design approaches. Crime activity can change from year to year. This may not be because the activity has increased, but because activity is reported inconsistently or because Police are targeting that particular crime activity.

BOCSAR Hotspot Mapping

A hotspot identifies crime density and is assigned a colour (red, orange or yellow) to reflect the strength of the hotspot. Hotspots are not adjusted to reflect the number of people living or visiting the location. This means they do not necessarily reflect areas with a higher than average risk of victimisation. This is why its important to refer to the actual counts of crimes in Table 1. However, hotspots can identify patterns, building or spatial typologies which are more impacted.

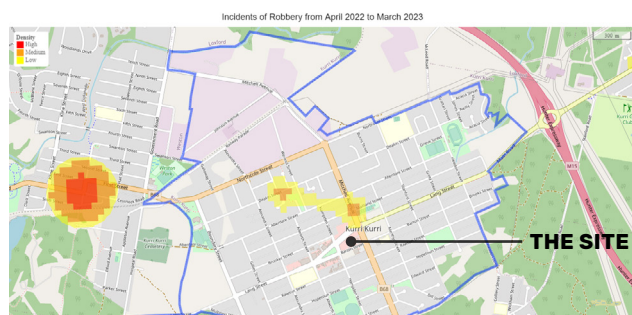


Figure 14: Hotspot - Robbery (BOCSAR2023).
The Development is not within a hotspot for Robbery.

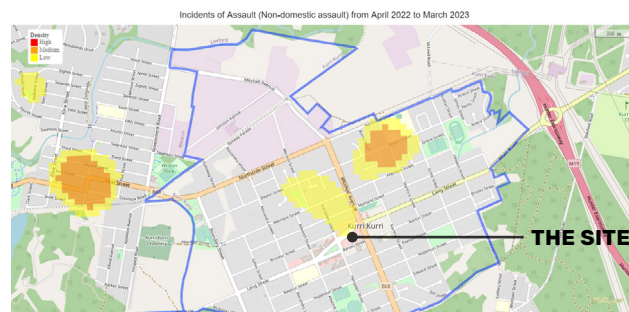


Figure 12: Hotspot - Non-Domestic Assault (BOCSAR2023).
The Development is on the edge of a Non-Domestic Assault hotspot. Hotspot is follows a consistent pattern which connects Lang St to Kurri Kurri Playground. In 2018 the hotspot was directly over the site.



Figure 13: Hotspot - Steal from Person (BOCSAR2023).
The Development is not within a hotspot for Steal from Person in 2023; however, the Development was within a hotspot in 2021.

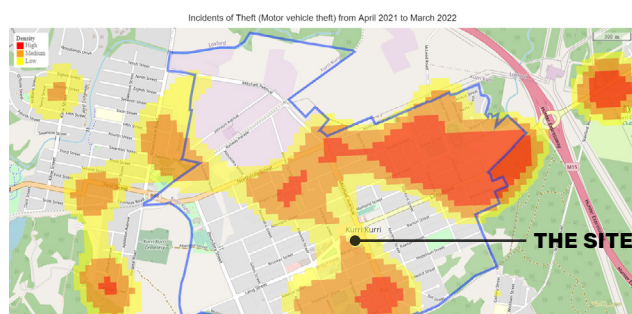


Figure 15: Hotspot - Motor Vehicle Theft (BOCSAR2023).
The Development is not within a hotspot for Motor Vehicle Theft in 2023 however, has been within a hotspot between 2016 - 2022. Map above is for 2022.

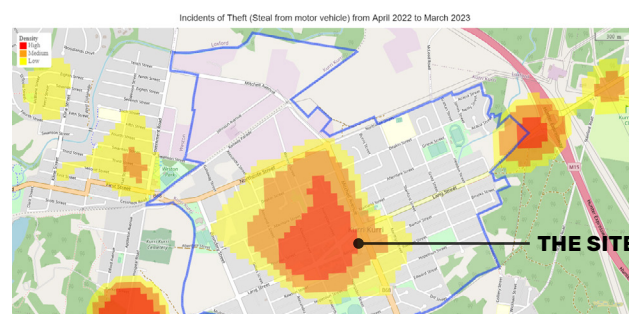


Figure 17: Hotspot - Steal from Motor Vehicle (BOCSAR2023).
The Development is within a Steal from Motor Vehicle hotspot. There has been a hotspot over the site since 2012 except for 2017 and 2022.



Figure 16: Hotspot - Break & Enter Non-Dwelling (BOCSAR2023).
The Development is within a Break and Enter Non-Dwelling hotspot. The hotspot affects the retail areas of Kurri Kurri. There has been a hotspot over the site since 2016, except for 2022.

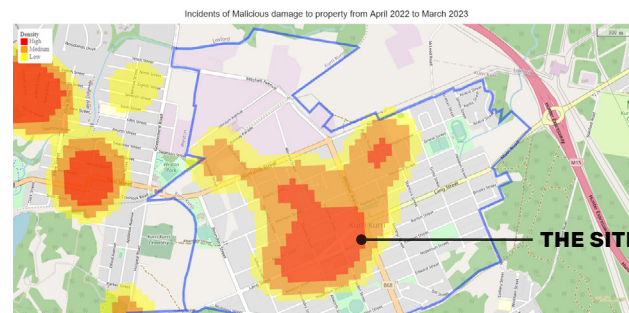


Figure 18: Hotspot - Malicious Damage (BOCSAR2023).
The Development is within a Malicious Damage hotspot. There has been a hotspot over the site since 2006 when hotspots were first recorded except for 2022.

Malicious Damage

Evidence of malicious damage was observed on and around the site. Malicious damage was in the form of illegal graffiti and tagging. All occur on large blank walls with surfaces that allow for graffiti to be visible and adhere. Most were located in highly visible locations while the laneway shown in Figure 23 is highly concealed and had the most 'layers' of graffiti.



Figure 19: Graffiti (TDP2023).

Illegal graffiti observed on the rear of the substation fronting Lang Street. The surrounding bushes create concealment opportunities.



Figure 20: Graffiti (TDP2023).

Illegal graffiti observed on the side of the carpark which is accessed from Lang Street.



Figure 21: Graffiti (TDP2023).

Graffiti on service building at rear of Kurri Kurri Hotel on the corner of Barton Street and Hampden Street.



Figure 22: Graffiti (TDP2023).

Graffiti on the western side of the existing shopping centre which will be demolished.

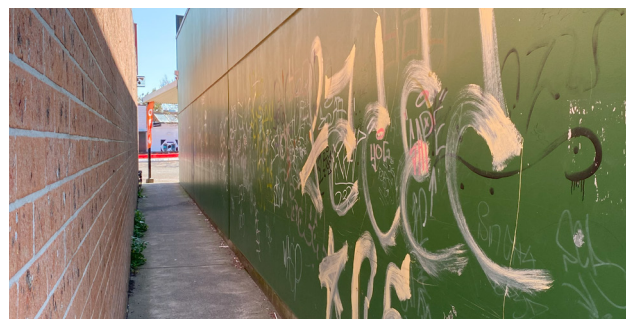


Figure 23: Graffiti (TDP2023).

Graffiti on the eastern side of the pedestrian laneway that connects the existing carpark to Lang Street. Note the affected side has the smooth surface, whereas the brick surface has less damage.

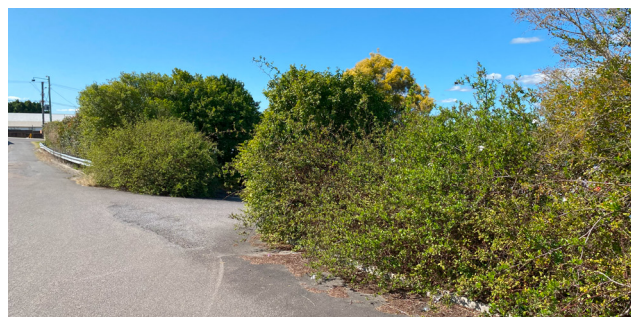


Figure 24: Concealing Vegetation (TDP2023).

Vegetation along eastern end of laneway provides concealment spaces. KFC is located below to the right of the image.



Figure 25: Hotspot - Malicious Damage (BOCSAR2023).

The Development is within a Malicious Damage hotspot. There has been a hotspot over the site since 2006 when hotspots were first recorded except for 2022

Murals

Murals often discourage malicious damage, such as illegal graffiti and tagging. Murals observed in the town centre were not damaged or tagged. This can be because tags can be difficult to see due to the artworks design and colour choice. It can also result from the respect accorded to artists, particularly when artworks are attributed, as most artworks in Kurri Kurri are. Should murals be part of the new development's strategy to mitigate graffiti, their design and colour choice should minimise large expanses of blank areas.



Figure 27: Mural - Amenities block
Mural on the facade of the Col Brown Rotary Park amenities block.



Figure 29: Mural Attribution
Example of attribution of artworks in the Kurri Kurri town centre which recognises the local story and the artist.



Figure 26: Mural - Drive-in
Mural on the western side of the existing supermarket.



Figure 28 Mural - Stairway to Heaven
Mural on the wall of the existing shopping centre, adjacent the Kurri Kurri Hotel.



Figure 30: Mural - Maypole by Greg Slevin
Mural on the side of a building in Lang Street. Attribution is visible on the wall to the right.

4.3 Social Impact Assessment - Community Consultation

A community consultation process was undertaken by Mara Consulting in June 2023. The consultation process was held between 30 June 2023 and 5 August 2023. The engagement approach for the project was guided by the International Association for Public Participation (IAP2) spectrum of public participation, delivered at an 'inform' and 'consult' level.

- emails including project information, how to provide feedback and an invitation to attend the drop-in session
- webpage with project information, how to provide feedback, survey link and event details
- drop-in information session
- survey.

The consultation process was undertaken following a Request for Information dated 7 May 2023, which stated that consultation should be undertaken given the "applicant did not undertake consultation prior to lodgement of the development application and therefore the Social Impact Assessment does not meet Council Guidelines". Council requested:

- targeted consultation with all affected property owners
- adequate community consultation, including those affected by road closure/opening during construction.

The consultation process included the following methodology:

- proponent-led one-on-one key stakeholder meetings
- establishment of project email address for enquiries and feedback
- letterbox drops to nearby businesses and neighbours
- emails to and meetings with Council and Councillors
- emails to and meetings with key stakeholders

As part of the methodology, a survey was undertaken. The outcomes are summarised in the Kingsway Plaza Consultation Report included in Appendix C. Overall, 96 percent of respondents believe the Kingsway Plaza redevelopment will be positive for Kurri Kurri and the region and 85 percent were very supportive of the redevelopment.

Summary of issues and responses

The following is a summary of the issues raised by stakeholders and the community during the consultation process. A CPTED response is provided that responds to each issue and how it can be addressed.

Issue	How addressed	CPTED Response
Council provided a DA request for information (RFI) which covered issues for consideration by the proponent.	A detailed response to the RFI will be provided to Council outlining how each item is being addressed by Higgins Planning. Please refer to the RFI document and technical responses from the broader project team.	A detailed response to the RFI is provided to Council. This CPTED report builds upon the CPTED letter lodged with the DA.
What will happen to the existing tenants?	Existing leases have come to an end and are currently running month to month. The existing tenants are welcome to make contact with the current leasing agents to provide their expression of interest to be future tenants.	Legacy tenants can continue to attract people to the new development.
What is the construction timeframe?	The process involves gaining development consent, preparing tender documents, tendering the project and appointing a principal contractor (builder), principal contractor preparing construction certificate documents and various supporting application under Roads Act and Local Government Act, demolition commencing, laneway being extended, laneway extension land being handed over to Council for a public road, existing laneway being closed, construction commencing on new building and associated works, construction process being concluded and occupation certificates being issued, Woolworths fit out being completed and store opening.	Protracted approval and construction programs can create CPTED vulnerabilities. For instance, vacant premises during the approval process and prior to construction reduce natural surveillance. Many premises were vacant during the site visit. Long construction programs also reduce natural surveillance as the area is no longer actively used.
Will there be other shops apart from Woolworths?	Yes, the original and amended drawings include Tenancy 1 (Spec T1) – 350m2 on upper level and unchanged in amended DA (no specific tenant known at this stage), Tenancy 2 (Spec T2) – 100m2 (no specific tenant known at this stage) on lower level and unchanged in amended DA, and Tenancy 3 (Spec T3) – existing pharmacy tenancy space reduced due to extended laneway to 200m2 on upper level fronting Lang Street.	Additional shops can benefit the development and its location, as they encourage a wider customer base. It has been identified that T1 could possibly be a cafe due to its location. A cafe will help activate both the street and the laneway, improving natural surveillance.
Will there be toilets?	An accessible water closet has always been labelled on the lower ground level "Acc WC" and is shown on the Amended Drawings lower ground level DA10.	Recommendations for the Accessible Amenities is provided in Sections 5, 7, 10 and 12.2. The amenities location has poor sight lines and could be vulnerable.

Issue	How addressed	CPTED Response
How many carpark spaces will there be?	In total there will be 177 car parking spaces: • 146 lower level • 31 upper level.	Ease of parking will encourage use of the development. Recommendations for the carpark spaces are provided in Sections 5, 7, 10 and 12.2
When will it be built?	Construction is expected to begin with 6 months of approval and subject to market conditions.	Vacant premises following the approval process and prior to construction reduce natural surveillance. Many premises were vacant during the site visit. Maintaining occupancy is recommended. If not practical, then pop up or temporary activation such as buskers can be useful.
How will cars get into the development?	As detailed in the Amended Drawings Access into the site is proposed from the extended laneway and Barton Street for vehicles	This report provides recommendations about securing the basement carpark in Sections 7, 10 and 12.2.
Questions were raised about traffic management and concerns about traffic impacts through construction.	If the Development Application is approved, a traffic management and safety plan will be developed. Traffic engineers have completed a traffic study and report as part of the Development Application and updated as part of the RFI process. The traffic management advice is reflected in the revised masterplan.	The traffic management plan and safety plan should incorporate CPTED principles.
Will the murals be replaced?	Yes. The project team has been working with Towns with Heart to identify an appropriate location for a new mural. The developer will consult with Towns with Heart to design a new mural.	Murals are useful in deterring graffiti. It is also important to continue the tradition of murals on the site as they support Territorial Reinforcement, and community connections to the development. Murals are encouraged to be located on walls at greatest risk of malicious damage. Recommendations are provided in Sections 8 and 12.2.
What security cameras and lighting will be in place?	Woolworths will have CCTV coverage for their entry and internal areas. There will be provision for coverage of the ground floor lobby and car park to be monitored back to the Woolworths base system. The basement and public areas will be lit in accordance with Australian Standards.	CCTV is recommended for the development. Section 6 and 12.2 provide recommendations.
Council raised concerns that there was insufficient consultation prior	In June 2023, Mara Consulting was engaged to support the request for further consultation. This report outlines the activities to consult and gather feedback to inform the revised development proposal.	Kristy Clanci of The Design Partnership was engaged in August 2023 to prepare a full CPTED report. This CPTED report considered the outcomes of the consultation process and integrated appropriate responses.

5 SURVEILLANCE

Surveillance is a core element of CPTED. There are three types of surveillance – Natural, Technical and Formal. The concept of surveillance seeks to discourage anti-social behaviour by creating an environment where people can see and interact with others. When people feel they are being watched, they are less likely to commit a crime.

Surveillance is achieved through well considered urban design, careful selection of landscaping solutions and lighting. This can create a safer environment. Successful surveillance outcomes are the product of good design.

5.1 Assessment

At this stage, detailed designs have not been prepared. This assessment and the following recommendations provide guidance for the construction certificate phase of the development.

5.1.1 Existing Conditions Assessment

The site comprises Kingsway Plaza, a two-storey neighbourhood shopping centre with frontage to Lang Street and Barton Street. Not all the premises are actively trading. The centre has frontage to Lang Street provided by three retail premises, and Barton Street provided by ten retail premises. Several premises have no or limited surveillance as glazing has been blocked with decals and signage, which blocks sight lines and surveillance opportunities. The existing carpark has limited surveillance from the surrounding buildings and is reliant on the users of the carpark. The existing landscaping is dense along the Barton Street boundary and the existing laneway and are concealment spaces.



Figure 31: Window treatments on Lang Street
Windows of Priceline block sight lines and restrict surveillance (GoogleStreetView2023).

5.1.2 Proposed Design Assessment

The development proposes a shopping centre comprising a Woolworths supermarket, specialised retail, a new plaza, loading areas and a carpark. The improved shopping centre may increase visitation resulting from the new through-site link and an enhanced shopping experience; however, the new development reduces sight lines and presents less activation to Barton Street in general. This requires a considered CPTED response, which is discussed below and forms the basis of the recommendations.

Plaza and T3 Speciality Retail

The new plaza creates a new pedestrian link and sight lines from Lang Street to the laneway and the proposed shopping centre. The proposed T3 Speciality Retail is likely to be a cafe which will significantly improve surveillance in this location, particularly if on street dining is provided. The plaza will have good sight lines down the laneway to Barton Street and Victoria Street.

Laneway precinct

The existing carpark will change and be replaced with the Woolworths shopping centre. The new building will shut down sight lines currently available across the carpark. Views to the entry of Woolworths and its open air carpark are partially concealed by the proposed planter in the carpark.

Open Air Carpark

The open air carpark is generally open and has reasonable sight lines however, the planters may create concealment areas and block sight lines.

Basement Carpark

The basement carpark has limited internal obstructions which can reduce sight lines (except for necessary columns).

Basement Carpark Entry

The basement carpark entry to the lift and traveller will benefit from the T2 specialised retail located at this level. The bottom of the traveller is concealed from view from half of the carpark due to the positioning of the amenities.

Carpark Amenities

The corridor to the amenities located in the basement carpark is concealed from view and has poor surveillance.

Lift

The lift has good surveillance from the Woolworths sentry. The Barton Street level has some surveillance from T2 speciality retail however, this will cease after it finishes trade at 6pm.

Loading Area

The loading area is a semi private space and will be managed when there are deliveries. At other times it will rely on technical surveillance.

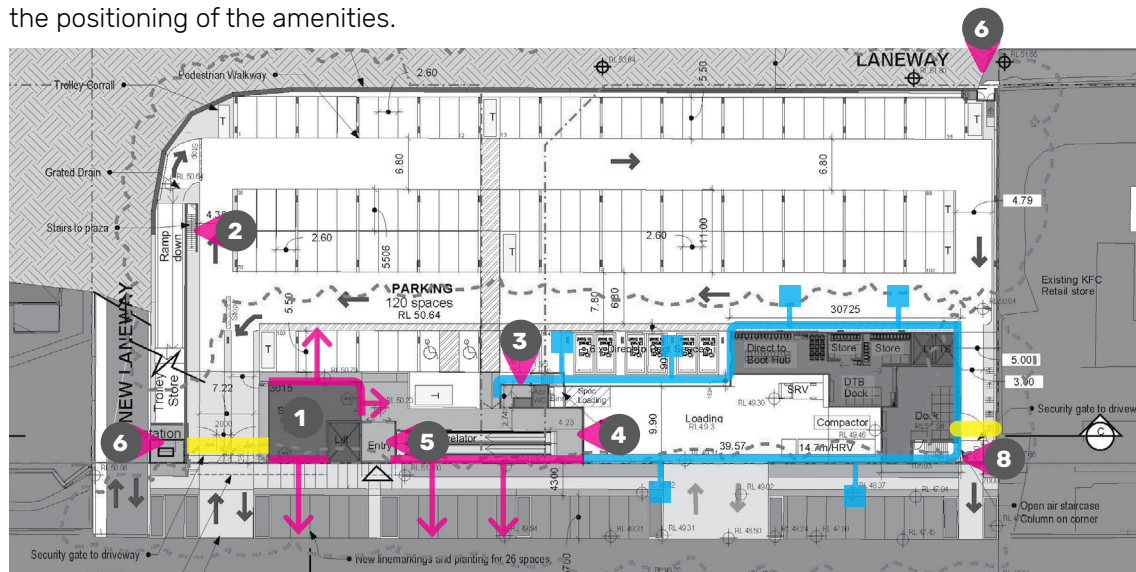
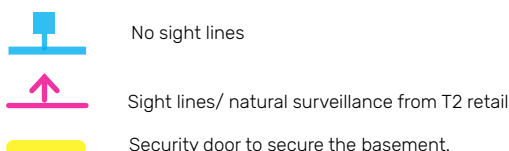


Figure 32: Basement Analysis



- 1 T2 specialised retail can provide some surveillance over the lift lobby and the entry to the traveller. This will rely on the glazing remaining clear and not obstructed by decals or signage.
- 2 The door to the stairs (from above carpark) are located at the bottom of the stairs. The stairway is become a concealment space.
- 3 Amenities corridor has poor sight lines and is a potential concealment space.
- 4 Entry to traveller is concealed from basement view lines.
- 5 Lift lobby is an alcove and a concealment space. A vandal resistant mirror may assist surveillance.
- 6 Concealment space resulting from the fire egress. Use of vandal resistant mirrors, lighting and CCTV will be required.
- 7 Substation has concealment areas behind and defensive landscaping solution are required.
- 8 Fire stairs are open and are vulnerable with limited natural surveillance.

5.2 Recommendations

The proposed development significantly improves the surveillance opportunities for the location.

5.2.1 Natural Surveillance and Sight lines

Sight line considerations are multi-directional. They are important for the public looking into the development and for those within the development looking out into the public domain.

Surveillance from the premises can generally be achieved for the plaza, open air carpark and sections of the Barton Street frontage. Natural surveillance and sight lines can be impacted by internal fitouts, window coverings and landscaping treatments and requires long term management.

Table 3 is a guide to assist in providing sight lines.



Figure 33: Example of impacted sight lines.

Sight lines blocked by decals, signage and displays such as the examples above are not encouraged.

Table 3: Sight lines

1.	<p><u>Window Treatments and Fixtures</u></p> <p>The application of decals, signage and posters on the retail glazing should be minimised so as not to obstruct sight lines.</p> <p>Seating, shelving and other internal built elements within specialised retail premises are encouraged to remain low or incorporate transparent elements to improve views. This allows for surveillance of both the streets, the new plaza and carparks. Use mirrors to monitor blind spots.</p> <p>While all retail tenancies should take the above approach, it is most important for T2 retail.</p>
2.	<p><u>Basement Carpark Amenities</u></p> <p>Entry to the carpark amenities are located in a concealed corridor. It is strongly recommended that the entry to the amenities be relocated to be directly off the traveller lobby space. This will increase the number of people with sight lines to the amenities, particularly the T2 retail while it is trading. Should this approach not be practical, then glazing the corridor wall may improve perceptions of surveillance.</p>
3.	<p><u>Internal Management</u></p> <p>Locate reception / sales desks, and other entry points into the specialised retail tenancies so they have good sight lines to the entry doors.</p>
4.	<p><u>Light Levels</u></p> <p>Sight lines can be impacted by light levels. Basement carpark lighting should be designed to maximise internal sight lines. Painting the walls, columns and ceilings of the carpark white can boost lighting outputs. Refer to Table 7.</p>
5.	<p><u>Landscaping for Sight lines</u></p> <p>Design and maintain landscaping to preserve sight lines. Refer to Table 5. The planters proposed in the open air carpark may block sight lines and create concealment spaces to the basement stairs. Reducing their height and setting back may assist.</p>
6.	<p><u>Window Placement</u></p> <p>The T3 retail which is likely to be a cafe has no windows oriented towards the supermarket. Additional windows could assist with both perceived and actual surveillance.</p>
7.	<p><u>Lift Lobby</u></p> <p>The lift lobby is a concealment and entrapment space. A vandal resistant mirror is recommended for the wall opposite the lift to assist with sight lines.</p>

5.2.2 Vegetation and Landscaping

The proposed development will benefit from new landscaping treatments in the street, new plaza and open air carpark. The design and installation of trees and landscaping can improve the amenity and CPTED outcomes for the development. However, if they are not designed, installed and maintained to CPTED principles they can create new issues.

Table 4 is a guide to assist with the planning, design and long term care of the landscape spaces.

Table 4: Vegetation and landscaping	
1.	Landscaping is recommended to be limited to ground covers, shrubs and taller trees. Lower tree limbs should be <u>above</u> average head height and shrubs should not provide concealment opportunities. Vegetation should not be concentrated in the middle zone between groundcovers, low shrubs and lower tree limbs. Refer Figure 34. The creation of pockets which facilitate concealment should be avoided by providing consistent planting and replacing dead plants which can result in gaps.
2.	Shadowy spaces are created where the trees obscure light fall from light poles and internal lighting. Consider the location of lighting and trees so as not to block the fall of light. This is particularly important in the plaza, entry to the lift, and stairs to the basement carpark where good lighting at night is important.
3.	Future advice on lighting design, in conjunction with the landscape design (during the construction documentation and construction phase) is recommended. This may include modelling of proposed lighting in conjunction with landscaping which is recommended to test light spill and shadows.
4.	Undertake a regular review of the vegetation and landscaping to ensure that it has not grown to create new or re-establish former spaces of concealment and entrapment.
5.	The substation on the corner of Barton Street and the laneway has a concealment area between the substation and the wall. Use defensive vegetation to discourage offenders from using as a hiding place.

5.2.3 Places of Concealment and Entrapment

Places of concealment and entrapment can be easily created in the public and private domain. These are spaces which may enable someone to hide or trap a person. These spaces can be formed through design decisions or are the result of vegetation growth and poor maintenance.

Table 5 is a guide to assist with the planning, design and long term care of buildings and landscape.

Table 5: Places of concealment and entrapment	
1.	Consider reducing the height and setting back the planter walls surrounding the stairs to the basement, as the planters and their vegetation are creating a concealment space.
2.	It is strongly recommended that the entry to the amenities be relocated to be directly off the traveller lobby space. This will remove the corridor that is a concealment space.
3.	As the design is developed, minimise alcoves and reassesses which can conceal a person. Where recesses are unavoidable, use lighting, splays and mirrors to reduce concealment opportunities.
4.	Provide good lighting in the basement where concealment spaces occur (if they cannot be reduced or eliminated).
5.	Use lighting, vandal resistant mirrors and CCTV in the fire stair alcove that opens onto the laneway.



Figure 34: Sight lines and landscaping

Example of tree and shrub clearance zones for good sight lines. The diagram shows a clear space between tops of shrubs and lower limbs of branches to provide clear sight lines.

6 LIGHTING & TECHNICAL SUPERVISION

Lighting and Technical Supervision refers to the role that lighting and products such as CCTV have in improving safety in the public domain. Many public spaces have lighting however, the specified products are often inappropriate, or are not wisely located. Often lighting creates new issues, as poorly located lighting can create shadows that previously didn't exist. Likewise, CCTV can often be a waste of money if there is low risk of crime, is poorly located or if no one is monitoring the footage. The following assessment and recommendations identifies methods to use lighting and technical supervision to achieve the best outcome.

6.1 Assessment

At this stage detailed designs have not been prepared. This assessment and the following recommendations provide guidance for the construction certificate phase of the development.

6.1.1 Existing Conditions Assessment

Existing lighting surrounding the development is identified as follows:

- Street lighting on Lang Street.
- Street lights on Barton Street.
- Very limited lighting in the existing carpark.
- No lighting in the existing laneway.
- Lighting for individual retail premises fronting Lang Street.

6.1.2 Proposed Conditions Assessment

Lighting design has not been undertaken at this stage and will be further developed during the construction documentation phase.

The development is proposed to trade after dark, including:

- Late night trading for the supermarket: The supermarket will be open until 12 midnight, 7 days a week.
- Specialised retail will trade until 6pm. It will be dark when it is not daylight savings.
- Open air carpark will be open 24/7, 7 days a week.
- Basement carpark will be open until close at midnight.

Therefore, the lighting design and maintenance program for the development will be important to assist with both actual and perceived safety.

The following recommendations can be used by the project architects and the consultant team to incorporate lighting and lighting enhancing features into their designs.

6.2 Recommendations

The guidelines should be considered in the design and management of the development:

Table 6: Lighting Design	
1. General	<ul style="list-style-type: none"> Ensure no shadowy spaces are created by the buildings, the gaps between the lights themselves and the landscape. Any signage should be as legible at night as it is during the day. Lighting strategy and design is recommended to be undertaken by a qualified lighting engineer for the development to test light spill and identify any spaces of shadow and concealment. Future advice on lighting design, during the construction documentation and construction phase is recommended. This may include modelling of proposed lighting in conjunction with landscaping is recommended to test light spill and shadows. Landscaping is recommended to be limited to ground covers, shrubs and taller trees. The landscaping should not provide concealment opportunities created by shadows. Trees and landscaping can block street and park lighting, forming shadows
2. Lighting and CCTV	<ul style="list-style-type: none"> The street, laneway and plaza should have effective lighting which improves visibility and as a result reduces fear. Bollard lighting is discouraged as it is prone to vandalism and is not particularly effective at illuminating faces. All lighting should comply with relevant Australian Standards, particularly AS 1158. Avoid using low-pressure sodium lamps. Vandal-resistant lamps are recommended. Avoid the use of coloured lighting Lighting can lose up to 20% capacity within 12 months, resulting in inadequate light levels. This should be considered when selected lighting type and brightness.
	<ul style="list-style-type: none"> Given the nature of the development, CCTV is recommended particularly at the Woolworths entry, in the open air carpark, in the laneway and plaza, lift lobby, footpath from Barton Street, basement carpark, loading area, and door to the basement amenities. CCTV and lighting should be considered in tandem to ensure the lighting outputs are adequate for usable footage. Monitor any other alcoves and deep recesses with CCTV.
3. Entries and other ingress/egress points	<ul style="list-style-type: none"> The entries to each retail tenancy should be bright and even to identify signage, the building name and number to assist with emergency services finding the building. Lighting inside and outside the entries should be designed to avoid mirroring which can obstruct sight lines. Design lighting to allow for eyes to adjust after leaving a transitional space – as it can take up to half an hour for eyes to adjust. The lift lobby requires lighting that is bright and even and reduces the opportunity for it to become a concealment and entrapment space.

7 TERRITORIAL REINFORCEMENT

Territorial Reinforcement recognises the importance of ownership of public spaces. Spaces that are well used and cared for are less likely to attract crime and anti-social behaviour. The role the community plays is often important to the success of the public space.

This principle seeks to remove the confusion from the public domain – to make the purpose of each space clear. This includes the definition of boundaries, function and operation of a space.

7.1 Assessment

At this stage detailed designs have not been prepared. This assessment and the following recommendations provide guidance for the construction certificate phase of the development.

7.1.1 Existing Conditions Assessment

The site currently comprises a supermarket, specialised retail and an open air carpark, over one lot. The building and carpark are under the control of the owners and tenants. The streets and laneway are controlled and managed by Cessnock City Council. The building and carpark and its trees form the boundary definition. The carpark is perceived to be public and appears to be used by people visiting the shopping centre and the other retail premises along Lang Street. The public either accesses Lang Street via the shopping centre or the pedestrian laneway; however, the pedestrian laneway is not as popular, which is likely due to its narrow width and graffiti, which can give poor perceptions of safety to some people.

7.1.2 Proposed Design Assessment

The new development will change the current Territorial Reinforcement framework of the site. The development will replace the existing ageing shopping centre and build over the existing open air carpark. While the current carpark has the perception of being public, the new carparking arrangement will appear more private. However, it is likely that people who visit the retail premises in the main street will

continue to park in the new carpark for convenience, particularly as the new plaza improves access.

The new development also formalises the laneway which is mostly unformed on its edges and appears to terminate at the existing carpark. The renewal of the laneway may encourage the retail fronting Lang Street to improve their interface with the laneway from both an activation and safety perspective.

The introduction of new specialised retail and hotel uses will require new management and maintenance requirements for the site. A more detailed analysis is provided of the spaces and their uses.

Management

As a whole, the development will be formally managed by Woolworths who is experienced in managing developments of this nature. They will be responsible for maintenance of the site and formalise how spaces will be used and managed. Individually, the staff will provide collective 'ownership' over the site and its shared spaces. Management processes should be documented in the Plan of Management.

Woolworths Supermarket

The Woolworths Supermarket occupies most of the site and will trade longer than other retail premises in the location. As a recognised brand, safety is expected when visiting the premises. These expectations also extend to its associated specialised retail and carpark.

Specialised Retail Premises

Three speciality retail premises are proposed within the development. These premises can create their own Territorial Reinforcement through door security, CCTV and separations between reception and staff areas. Ownership and sight lines can be achieved through appropriate glazing treatments, such as using transparent elements and limiting areas covered by decals and signage.

Amenities

The amenities are under the control of building management. T2 retail premises in the basement carpark will have a level of community control of the amenities as they are located in the same space and can have a line of sight to the amenities. The amenities are recommended to be closed at 6 pm when the T2 specialised retail closes – as once T2 closes, the space will have reduced natural surveillance. Longer operating hours will require formal supervision or the relocation of the amenities to the Woolworths supermarket level. Access Control for these amenities is discussed in Table 7 and Section 10 Access Control.

Carparks

The open-air carpark is likely to have a strong territorial relationship with the supermarket as they are located on the same level. Due to the convenience of being on the same level and possibly higher perceptions of safety, the open-air carpark may have higher visitation in the evening. This could reduce the natural activity in the basement, making it more vulnerable.

Security of the basement carpark is critical in this area, for both vehicle theft and steal from vehicle. Other crime activities, including non-domestic assault, steal from person, sexual assault and malicious damage, are also recorded in the area code and are well above the NSW average. Due to the late trading hours of the Woolworths premises, Territorial Reinforcement of the carpark will need to be well designed and managed. Customers, staff and

the general public may be vulnerable in the evening when they walk to their cars in the basement.

Wayfinding is important in carparks. Some offenders prey upon people that appear lost or confused. Directional signs should not be located near concealment points or direct people past concealment points. Use colour and motifs to identify the entry to the traveller and lift.

Loading areas and back of house

The loading areas and back of house are located under the supermarket and will be accessed from Lang Street. These spaces are proposed to be secured and will not be accessible by the public.

Separation of Uses

The separation of public and private uses is important in establishing natural Territorial Reinforcement. Separation areas include:

- public (general public limited to Woolworths supermarket, the specialised retail premises and amenities); from
- semi-private uses (back of house where deliveries and waste pickups are undertaken),
- private uses (staff areas such as the Mezzanine, parts of the loading area, specialised retail premises staff areas).

7.2 Recommendations

Table 7: Territorial Reinforcement	
General	
1.	<p><u>Lighting</u></p> <p>Lighting can be used to reinforce entry points to the building including the Woolworths entry, lift lobby, carpark entries and specialised retail entries</p>
2.	<p><u>Maintenance</u></p> <p>Maintenance of the development is a powerful tool in Territorial Reinforcement. Well care for buildings and their landscape demonstrates that it is cared for and that people are watching. A Maintenance Plan should form part of the Plan of Management. The Maintenance Plan should include the selection of durable materials and plant species that do not require extensive maintenance.</p>
3.	<p><u>Working Group</u></p> <p>Establish a working group comprising Woolworths staff, specialised retail tenants, neighbouring retail (such as those backing onto the lane and on Barton Street), Kurri Kurri hotel and security to identify issues and incidents and to identify possible solutions. Council and local police are recommended to be part of the group as required.</p>
4.	<p><u>Construction Phase</u></p> <p>Measures to secure the site during construction are required. A CPTED construction plan should be established prior to commencing construction. Actions include:</p> <ul style="list-style-type: none"> Store tools and building materials in strong rooms with tamper proof security systems.
5.	<p><u>Loading area and back of house</u></p> <p>The loading areas and back of house are semi-private and private and this should be clearly expressed through signage and Access Control. A change in material/colour on the loading area floor is recommended to distinguish it from more public areas such as external footpaths. Secure all loading areas and back of house when not in use.</p>
6.	<p><u>Amenities</u></p> <p>The location of the amenities are likely to have low levels of natural surveillance in the evening, despite being in an area managed by the centre. It is recommended that they be secured when the T2 specialised retail closes at 6pm. Should the amenities be required to stay open past that time, then formal supervision may be necessary by security patrols. Refer to Access Control.</p>
7.	<p><u>Signage</u></p> <ul style="list-style-type: none"> Signage should be used to clarify ownership and responsibility. In the case of damage, signage should assist the public in reporting damage. Clear signage should be installed throughout the development. Overuse of signage is not supported as it becomes less effective. Consistent design will add to the overall sense of a cohesive community and sense of place. The street name and building name should be clearly visible from the street and laneway and be illuminated. Directional signage to T2 is recommended from the Woolworth level. A map of the building is recommended to assist with wayfinding. Signage should not cover windows and block sight lines to the carparks, street, laneway or plaza. Directional signs should not be located near concealment points or direct people past concealment points. Use colour and motifs to identify the entry to the traveller and lift
8.	<p><u>Basement carpark</u></p> <ul style="list-style-type: none"> The basement carpark needs to be capable of being secured in the evening when the centre closes. This should be for both vehicles and pedestrians. While vehicle theft is an important consideration, it is the risk of thefts from vehicles, malicious damage and assault that pose a risk to the users of the basement and the basement itself. Gates/doors should not be a boom gate and should prevent pedestrian access. Park Smarter signage is recommended in the basement
9.	<p><u>Basement Carpark Stairs</u></p> <p>The stairs to the basement are located in the open air carpark and the door is proposed at the bottom of the stairs. The stairs are out of sight and will not have visual supervision from the supermarket, unless there is formal supervision. It is recommended that the door to the stairs be relocated to the top of the stairs. CCTV will be required in this stairwell.</p>
10.	<p><u>Help Point</u></p> <p>Install help points/phones in the basement carpark and in the Woolworths forecourt. If the lift is to operate 24/7 then the Help Point should connect to security</p>

8 ENVIRONMENTAL MAINTENANCE

Environmental Maintenance is the maintenance and management of physical assets. This takes place during the operational stage of the Precinct. The assessment and recommendations provided below are guidance for the current design process and for future management. Public spaces that are poorly maintained, damaged, vandalised, appear abandoned and full of rubbish are unlikely to attract positive user groups. These types of spaces show a lack of pride by the community. Well maintained spaces send a positive message and tells potential offenders that people are watching and using this space. It is a good deterrent. Good design can help with maintenance in the future.

8.1 Assessment

Planning for Environmental Maintenance will generally occur in the operational stages however, there are opportunities to integrate solutions during the construction certificate phase which will assist in the management of the new development. The aim is to create a development that is easy to maintain.

8.1.1 Existing Conditions Assessment

At present, the site comprises a supermarket, retail and an open air carpark, managed by the owner(s) and tenants of the buildings. The building and the neighbouring premises are aging and most have been vandalised through illegal graffiti and tagging. The landscaping along the edge of the carpark and the lane are healthy but contribute to the informal character of the precinct and reduce surveillance. The buildings proposed for demolition have murals which form part of the Kurri Kurri mural collection. It was observed that the murals in the town are not affected by tagging which is localised to blank walls.

8.1.2 Proposed Design Assessment

The proposed development presents an opportunity to provide a well maintained development and precinct that benefits the local community.

Due to the nature of the development typology - big box retail space on a sloping site - it results in retaining walls and floor levels above the footpath and lane. Some of these walls may be vulnerable to malicious damage. The carpark is also at risk due to its isolation and large blank walls.

8.2 Recommendations

Table 8: Environmental Maintenance

Design Stage	
1.	Reducing risk of malicious damage <ul style="list-style-type: none"> Walls at the street, plaza and laneway are at risk of malicious damage. Select wall finishes that make it hard to identify a tag as this discourages tagging. The use of anti-graffiti coatings is recommended. The northern wall facing the laneway will be vulnerable due to poor natural surveillance. A mural that contributes to the local mural collection is recommended and could deter graffiti. The mural proposed on the east facade is a good location. Avoid the use of porous materials in areas with the greatest risk of graffiti tagging.
2.	Select materials that are robust and durable. Materials/elements should not be easily removed.
3.	Provide rubbish bins in the plaza, Woolworths entry and basement carpark and plan for regular removal to avoid build up of waste. Bins should be flame proof.
4.	Specify low maintenance and drought-resistant plant species.
Management & Operational	
5.	Establish an Environmental Maintenance Plan as part of the Plan of Management.
6.	Establish a reporting system that allows users and visitors to quickly report damage or anti social behaviour.
7.	Repair or replace broken or damaged furniture/bins/signs quickly
8.	Establish a rapid removal program for illegal graffiti and tagging.

9 ACTIVITY & SPACE MANAGEMENT

Activity and Space Management identifies strategies to establish natural community control.

These strategies include:

- Formal supervision
- Control of the space
- Maintenance of the space.

Activity and Space Management has strong correlations to earlier sections of this report. For instance, poor Environmental Maintenance results in urban decay which sends a negative image. This section explores how to improve community safety by increasing visitation and use of a space.

9.1 Assessment

Activity and Space Management will generally occur in the operational stage however, there are opportunities to integrate solutions during the construction certificate phase which will assist in the management of the new development. The aim is to create opportunities for natural community control.

9.1.1 Existing Conditions Assessment

The management of the subject site will change following the construction of the new supermarket development. The new mix of customers, public visitors and staff, and a new building typology will require a higher level of Activity and Space Management.

9.1.2 Proposed Design Assessment

The development will comprise a combination of public, semi-private and private spaces. The Woolworths management company will play a key role in the functionality and operations of the development. While some areas of the development will have strong natural community control – such as the plaza, Woolworths, T3 retail, and the open air carpark – the basement carpark is more vulnerable as it will rely on formal supervision (security guards) and mechanical supervision (CCTV). While CCTV has a vital role, it is not a proactive strategy and is most useful after the incident. There are opportunities to strengthen the carpark design to reduce the instances of becoming a target. The Plan of Management will need to address CPTED matters in the development's operations.

9.2 Recommendations

Design should enforce clarity of land use – making it clear what is public (where the public can go), semi-private (where delivery and waste personnel can go) and what is private (where retail staff can go). Maintenance of the space sends strong messages that can either support or undermine safety.

Table 9: Activity & Space Management

1.	Use different paving treatments between different spaces within the development such as differentiating the footpaths from fire exits and loading areas. Define the pedestrian entries to the development with entry features, structures, signage and lighting.
2.	Clearly identify areas which are not accessible to visitors such as storage areas and waste rooms. However, be cautious of the over use of signage as it may become less effective.
3.	Use CCTV, signage, controlled access (swipe card, pin or eqv.) to manage public access.
4.	Provide security patrols for the public amenities while the premises are trading.
5.	Provide security patrols for the lift if it remains open 24/7. Refer to Section 10. Access Control.
6.	Develop a Plan of Management that integrates CPTED guidelines for surveillance, access control, hours of operation and maintenance recommendations. Review the current trends in crime in the area with the local Police and regularly update Plan of Management. Establish a Maintenance Plan, forming part of the Plan of Management, that ensures the site remains clean and well maintained.
7.	When the traffic management and safety report is prepared, incorporate CPTED principles.

10 ACCESS CONTROL

Access Control identifies methods to manage access to and within a site. Methods may range from site restrictions, through to helping people leave a site safely.

Three methods are generally used: Natural Control, Technical or Mechanical Control and/or Formal Access Control. Natural Control Solutions seek to restrict and channel people into specific areas. Restrictions/channelling can be a good deterrent for criminals as it increases the risk of being caught.

Methods are typically Natural Control solutions which may include:

- Landscape site planning such as fencing, water features, paths and vegetation
- Building site planning
- Wayfinding signage
- Control of lines of sight

Technical or Mechanical Control Solutions may include the use of Closed Circuit Television (CCTV) equipment. Lastly, Formal Access Control solutions would be the use of security officers.

10.1 Assessment

Access Control is determined during the design and operational stages, which will assist in the development's management. While Access Control can be added at any time, many design decisions can make Access Control easier or harder to manage the facility on a day to day basis or may require future renovations. The following items can be introduced during the construction certificate phase.

10.1.1 Existing Conditions Assessment

At present, the site comprises a shopping centre composed of a supermarket and specialised retail. The retail premises are accessed from within the centre and public access is limited to two public doors. Each of the individual premises within the centre have their own Access Control.

10.1.2 Proposed Design Assessment

Woolworths Shopping Centre

The Woolworths centre has several different spaces and functions – some areas that are public and some that are private. The public and private areas are separated. The building's typology has been developed over a long period and repeated in different locations across the country, with variations

that respond to its context. In this instance, the site's topography lends itself to a basement carpark. Access Control considerations for the basement carpark are discussed below.

Specialised Retail Access

The specialised retail units are individually secure, and all have external access except T2, located in the basement carpark, which also has internal access where one door opens into the carpark.

Amenities

Amenities are located in the basement carpark adjacent the traveller. Section 5 recommends the amenities be reoriented to open into the traveller lobby to remove the concealed access corridor. These amenities are also recommended to be locked at 6pm when T2 retail closes. A key can be provided for Woolworths staff. Due to their isolated location, the amenities (and the basement carpark) are recommended to be monitored by security from 6pm until close at midnight. Periodic checks throughout day time trading would also be beneficial. CCTV should be positioned to monitor the entry to the amenities.

Basement Parking

The basement carpark will need to be capable of being secured to prevent pedestrian access when the centre closes at 12 midnight. This is for both

vehicles and pedestrians, as pedestrians pose a risk of malicious damage and anti-social behaviour. This requires the ramp and stairs from the open air carpark to the basement carpark to be secured. The door to secure the stairs is currently located at the bottom of the stairs and is creating a concealment and entrapment space. A door is proposed at the Barton Street vehicle entries.

While bike parking has not been nominated it may be included in the future for customers and staff. Customer (short-term) bike parking should be located near the entry with good sight lines from Woolworths. Staff parking, if located in the basement carpark is recommended in individual secure and transparent lockers as bikes and bike parts are often stolen and opaque lockers can be used to store illegal items.

Travellator

Access to the basement via the travellator is not secure after hours. A temporary concertina gate is proposed to enclose the top of the travellator at close each night. Preference is given to a more permanent solution which prevents access.

Lift access

There is a recommendation by Council for the lift to remain open 24/7, 7 days a week to provide a mid-block connection between Barton and Lang Street. This is not supported as the space could experience anti-social behaviour, malicious damage and public urination. It is preferable for the lift to be secured when Woolworths ceases trade at 12 midnight. It will also assist in enabling the travellator to be secured in the evening with a glazed lobby.

Should Council require 24 hour access, then further investigations are recommended to identify who will be using the lift after midnight, where they travelling to and is 24 hour access necessary or it increasing the risk in the area. Security patrols are recommended after the centre closes at 12 midnight

until Woolworths opens at 6am the following day. Long term consultation with local police is strongly recommended to monitor the lift and change its hours of operation if required.

Fire Stairs

Fire stairs are opportunities where access can be illegally obtained, for instance by a fire door propped open by a resident or staff member. Alarming fire stairs is recommended.

The fire stair that exits onto the laneway is concealed could be propped open and provide internal access to Woolworths above.

The fire stairs on the south-east corner are proposed to be open which provides access to the Woolworths floor above. This area is a concealed space and could provide an opportunity to break into the Woolworths premises, and graffiti the stairwell. If it is to remain 'open' the stairs need to be secured to prevent access.

Back of House

The back of house and service areas are proposed to be secured to prevent access by the public. Clear signage and CCTV is recommended.

10.2 Recommendations

Table 10: Access Control	
1.	<p><u>Basement Carpark Access</u></p> <ul style="list-style-type: none"> Secure the basement carpark entries from Barton Street with full height doors that prevent vehicular and pedestrian access. Secure the ramp from the open air carpark to the basement with a gate that prevents vehicular and pedestrian access. The door should be at the top of the ramp so as not to create a concealment space that could be used by offenders and malicious damage. If a temporary concertina gate is used to secure the top of the traveller at close each night, the temporary gate will need to fully enclose the traveller so access cannot be gained on the side. It should not also not facilitate climbing over the gate to gain access into the basement. Preference is given to a more permanent solution which prevents access.
2.	<p><u>Lift</u></p> <ul style="list-style-type: none"> Secure the lift when Woolworths ceases trade at 12 midnight. This will allow for the traveller and lift to be secured in the evening within a glazed lobby. Should Council seek to recommend 24 hour lift access, then further investigations are recommended by Council to identify who will be using the lift after midnight and where are they travelling to ensure the risk is necessary? If 24 hour access is required, provide security patrols in the evening until Woolworths opens at 6am the following day. Undertake long term consultation with local police to monitor the lift and reduce its hours if crime takes place. Undertake daily cleaning of the lift and its alcove to remove any waste, sharps, or graffiti.
3.	<p><u>Amenities</u></p> <p>Secure the amenities at 6pm when T2 Specialised Retail closes. Provide a key for staff and monitor with security patrols.</p>

4.	<p><u>Fire Doors</u></p> <ul style="list-style-type: none"> Use door closers and alarms on fire exits to ensure fire doors are not left open. Use signage to warn people of penalties in propping doors open which leaves the building unsecured. CCTV is proposed around these exit points. Secure the fire stairs that exit onto Barton Street. If the intent is for the stairs to remain 'open' the gate should be full height not facilitate climbing.
5.	Regularly review the Plan of Management to address issues as they arise.
6.	All external door hinges to be mounted so they cannot be removed.
7.	Avoid creating natural ladders into the development from adjoining properties. The design of the building should not facilitate climbing.
8.	Windows require quality locks and unused windows must be closed and sealed permanently, particularly when in concealed locations. Skylights to be secured. Windows should be capable of being locked in a partially open position eg. bolt lock. External windows and doors to be of solid construction.
9.	Secure and limit access to the waste room and building services. The waste room should not be accessible by the public.
10.	Construction sites to be secured and locked. Provide CCTV. Building materials stored in strong rooms, Security needs to be tamper proof.

11 3 D's : DESIGN/DEFINITION/DESIGNATION

It is important for the design, definition and definition of a public space to be in harmony. If a space comprises uses that conflict with another use, dangerous situations could arise. Offenders often exploit situations that are confusing.

This can be described as the three D's.

Designation

- What is the designated purpose of this space?
- What this its original purpose?
- How well does the space support its current or its intended use?
- Is there a conflict between its current and intended use?

Definition

- How is space defined?
- Who owns the space and is it clear?
- Where are its borders?
- Is the space defined by social or cultural factors – does this affect how space is used?
- Are the legal or administrative rules clearly set out and reinforced in policy?
- Are there signs?
- Is there conflict or confusion between designation and definition?

Design

- Does the physical design support the intended function?
- Does the physical design support the desired or accepted behaviour?
- Does the physical design conflict with proper functioning of the space?
- Is there confusion or conflict in the physical design which is intended to control behaviour?

11.1 Assessment

If the recommendations in previous sections are carried out it will be clear which spaces are semi public and which are semi-private.

Ongoing maintenance by strata and a clear plan for maintenance work will be beneficial.

Spatial boundaries throughout the site will reinforce intended function if the recommendations in this report are carried out.

11.2 Recommendations

- Undertake a 3 D's assessment regularly.
- Implement recommendations from Sections 5 – 11.

12 CONCLUSION

12.1 Summary

The proposed development will change the site's current use - from an ageing shopping centre and open-air carpark to a new shopping centre providing a Woolworths, specialised retail, offices, loading area, plant and associated car parking. Analysis of BOCSAR data for the locality identifies several hotspots at the site for steal from vehicles, break-and-enter non-dwelling, and malicious damage. The rates for most crime activities is significantly above the NSW average and are of concern for the proposed development. The nature of the crimes is relevant to this assessment as the development will contain retail premises and many vehicles in public arrangements - all of which could become targets.

As steal from vehicle is of concern in the area, the carpark will be vulnerable. Steal from vehicle requires the control of pedestrians, not just vehicles. This means physical barriers and not boom gates, which only restrict the movement of cars. Non-domestic assault and sexual assault is also a concern, and managing this risk through methods such as eliminating concealment spaces, security patrols, good quality lighting, CCTV, maintenance and access control (including hours of access control) will need to become part of the long term management of the building and form part of a Plan of Management. The Plan of Management should be prepared prior to the issue of the Construction Certificate. Malicious damage is recorded at over double the state average, and it was identified on site and in the surrounding area. Opportunities to reduce malicious damage, such as illegal graffiti, through material selection, the use of murals, supervision and access control are strongly recommended.

This project's key recommendations are provided on the following pages and summarise the key issues, opportunities and identify further things to consider. The documentation is at a Development Application level. Therefore, many specific details are yet to be fully established and will be documented during future stages. However, this report endeavours to provide recommendations to guide the plans for future design development to ensure it is set in the right direction. The key recommendations address all principles, including Surveillance (including Lighting), Territorial Reinforcement, Environmental Maintenance, Activity and Space Management and Access Control. The proposed design has the opportunity to be attractive and will contribute to the evolving nature of Kurri Kurri.

A high level of consistent maintenance and the delivery of high-quality finishes will demonstrate to patrons and visitors that this is a well-loved development - showing there is guardianship over the development. Therefore, it is essential that there is a strong visible presence of people and the limiting of opportunities for criminal behaviour so there is no perception of vulnerability which could turn this site into a target. Consultation with local area police is highly recommended to monitor the area and the proposed development.

12.2 Key Recommendations

The recommendations and guidelines from the CPTED principles in Section 5 - 11 should be used to guide the construction certificate phase and management of the development.

The following are the key recommendations and ongoing considerations. This summary must be read in context with each section of this report and the supporting analysis, which provides the full range of recommendations for the project.

Lift Access

There is a recommendation by Council for the lift to remain open 24/7, 7 days a week to provide a mid-block connection between Barton and Lang Street. This is not supported as the space could experience anti-social behaviour, malicious damage and public urination. It is preferable for the lift to be secured when Woolworths ceases trade at 12 midnight. It will also assist in enabling the traveller to be secured in the evening with a glazed lobby.

Should Council require 24 hour access, then further investigations are recommended to identify who will be using the lift after midnight, where they travelling to and is 24 hour access necessary and worth increasing the risk in the area. Security patrols are recommended for the lift after the centre closes at 12 midnight until Woolworths opens at 6am the following day.

Long term consultation with local police is strongly recommended to monitor the lift and change its hours of operation if required.

Access Control of the basement

The basement carpark will need to be capable of being secured to prevent pedestrian access when the centre closes at 12 midnight. This is for both vehicles and pedestrians, as pedestrians pose a risk of malicious damage and anti-social behaviour.

This requires the following:

- The door to secure the stairs is currently located at the bottom of the stairs and is creating a concealment and entrapment space.
- Secure the basement carpark entries from Barton Street with full height doors that prevent vehicular and pedestrian access.
- If a temporary concertina gate is used to secure the top of the traveller at close each night, the temporary gate will need to fully enclose the traveller so access cannot be gained on the side. It should not facilitate climbing over the gate to gain access into the basement. Preference is given to a more permanent solution which prevents access, such as a glazed lobby.

Should crime take place in the basement carpark once it begins to trade, it may be reasonable to close the basement carpark earlier in the evening. Customers for Woolworths can use the open air carpark which has natural surveillance from the supermarket.

Access Control for Woolworths

Secure the open fire stairs that discharge next to the eastern basement driveway. This egress has access to Woolworths above and is a concealment space. The door should be full height and not enable it be climbed over.

Access Control, Surveillance and Territorial Reinforcement of the Amenities

Entry to the carpark amenities are located in a concealed corridor. It is strongly recommended that the entry to the amenities be relocated to be directly off the traveller lobby space. This will increase the number of people with sight lines to the amenities, particularly the T2 retail while it is trading. Should this approach not be practical, then glazing the corridor wall may improve perceptions of surveillance.

The amenities are recommended to be locked at 6pm when T2 retail closes. A key can be provided for Woolworths staff. Due to the amenities isolated location, the amenities (and the basement carpark) are recommended to be monitored by security from 6pm until close at midnight. Periodic checks throughout day time trading would also be beneficial. CCTV should be positioned to monitor the entry to the amenities. Another solution is to relocate the amenities to the Woolworth level where they can be monitored.

Minimising risk of Malicious Damage and ongoing management

Malicious damage has been identified in the report as a high risk for the development. Malicious damage can give the perception that an area is unsafe and discourage visitation. The reduction of visitors to a place will in turn reduce natural surveillance. The ongoing lack of natural surveillance creates the opportunity for more malicious damage. This can become cyclical cycle that can be hard to break.

There is a large number of blank walls, retaining walls and fences both externally and internally which could be targets. As observed throughout the town centre, the targeted walls are surfaces that make graffiti and tagging easy to see and apply. There are three approaches and a combination is recommended for this development:

- 1. Murals:** Establish murals on walls most at risk of malicious damage, such as along the laneway and on Barton Street. Target walls that are either at pedestrian height or are accessible by climbing onto a structure, such as a wall or fence. The murals should avoid large blank areas and use patterns, motifs and forms that make it hard to see illegal graffiti. Provide attribution that tells the artwork's story and the artist's name, as this is often respected and reduces attacks. Consider using local young artists to build ownership and connection to the artworks. Another option is to

establish a temporary legal graffiti wall that the youth use to grow their artistic skills. This project could be run in conjunction with Council.

- 2. Wall Finishes:** Select wall materials and finishes that make it hard to see and apply graffiti. The existing pedestrian lane is a prime example of how different wall finishes encourage or discourage graffiti. Avoid porous surfaces that make it hard to remove graffiti.
- 3. Rapid Removal:** Adopt a rapid graffiti removal program for the site. Remove all graffiti as soon as possible, as this can discourage further attacks. Use anti-graffiti coatings that make it easier to remove. This program could grow to include the surrounding businesses to improve the precinct as a whole and improve perceptions of safety.

Building natural community guardians

Developing a strong network of natural community guardians in the area could be beneficial. These guardians would include the managers and staff of the new development, along with surrounding retail owners, tenants and residents. The goal is to work together to identify issues and come up with solutions.

The following is a summary of the recommendations:

- Establish a working group to identify problems and solutions as they occur.
- Work with Council to activate the laneway and the plaza through property improvements that address the lane rather than turn their back to it, undertake mural projects and consider laneway events.
- Install a busker or similar in the plaza who provides natural surveillance over the laneway and guardianship. Particularly as there are few guardians with surveillance over the laneway.

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APPENDIX A - DESIGN DRAWINGS



DRAWING LIST

DA01 - Cover Sheet	B
DA02 - Site Plan	B
DA03 - Land Acquisition Plan	B
DA04 - Demolition Plan	B
DA05 - Landscape Plan	B
DA06 - Site Staging Plan	B
DA07 - Public Domain: Lang St	B
DA08 - Public Domain: Barton St	B
DA10 - Lower Ground Plan	B
DA11 - Ground Plan	B
DA12 - First Floor Plan	B
DA13 - Roof Plan	B
DA20 - South and East Elevations	B
DA21 - North and West Elevations	B
DA22 - Sections AA + CC	B
DA23 - Detail Sections	B
DA25 - Window Schedule	A
DA26 - Signage Schedule	B
DA30 - Shadows 9am June 22nd	A
DA31 - Shadows 12pm June 22nd	A
DA32 - Shadows 3pm June 22nd	A
DA35 - Materials Schedule	B
DA36 - GFA Plan	B

DEVELOPMENT APPLICATION

VOTRAINT NO124 PTY LTD
PROJECT NO: 2201



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PROJECT TITLE:
174-178 LANG STREET, KURRI KURRI
ISSUED FOR
DEVELOPMENT APPLICATION


Drawing Title
Drawing Scale
Issue Date
Revision

DRAWING LIST
NTS
21/08/2023
B

Drawing

DA01



VOTRAINT NO124 PTY LTD PROJECT NO: 2201	 NORTH	This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Finished ground levels on the plan are subject to site conditions. Do not scale from drawings. Figured dimensions are to be given preference over scale.	PROJECT TITLE: 174-178 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title Drawing Scale Issue Date Revision	SITE PLAN 1:1000@A3 21/08/2023 B	Drawing DA02
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VOTRAINT NO124 PTY LTD
PROJECT NO: 2201



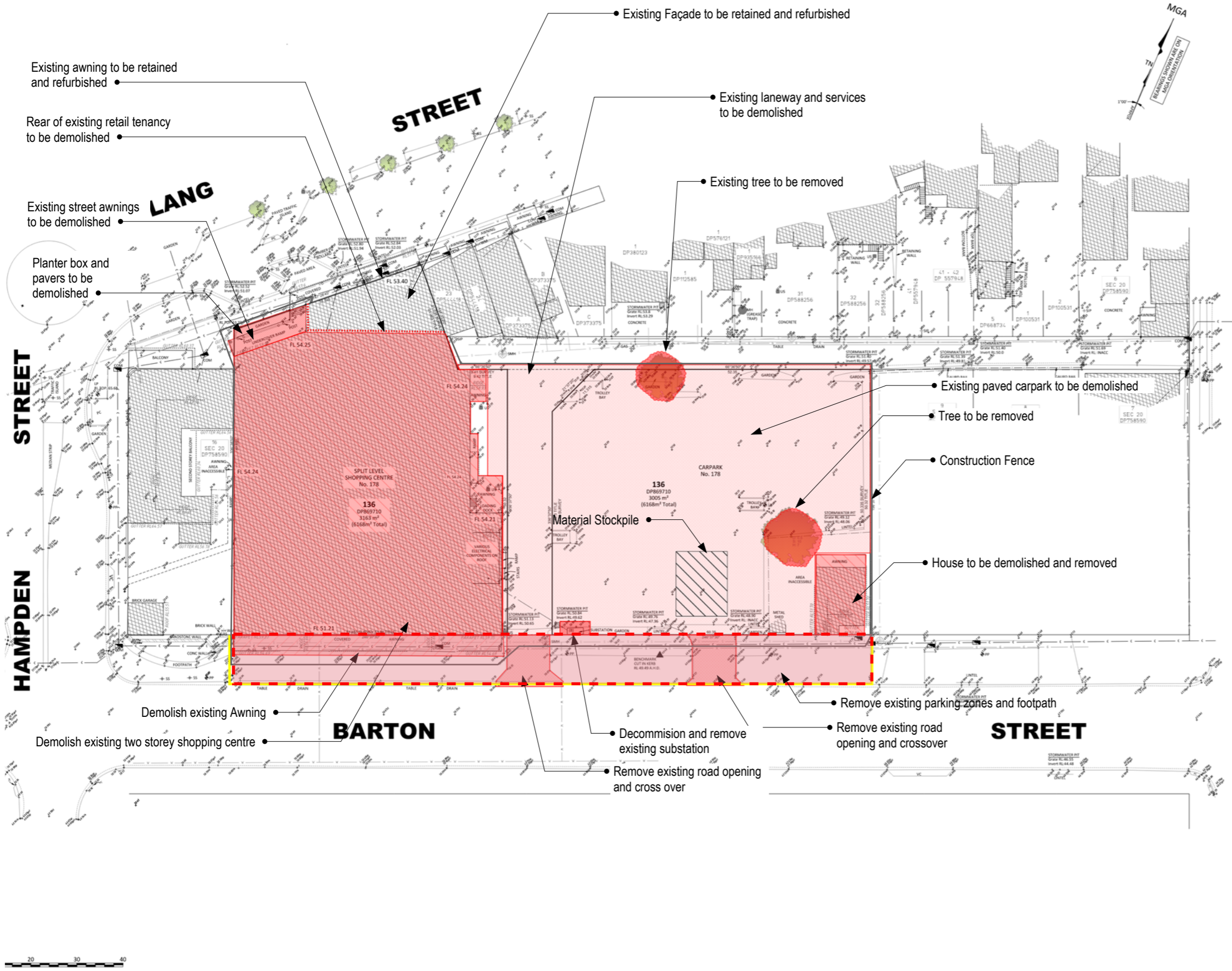
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PROJECT TITLE:
174-178 LANG STREET, KURRI KURRI
ISSUED FOR
DEVELOPMENT APPLICATION

Drawing Title
Drawing Scale
Issue Date
Revision

SITE ACQUISITION PLAN
1:1000@A3
21/08/2023
B

Drawing
DA03



DEMOLITION NOTES

CONTRACTOR TO CONFIRM EXISTING CONDITIONS ON SITE.

CONTRACTOR TO DEMOLISH EXISTING STRUCTURE/REMOVE ALL MATERIALS. CUT AND SEAL SERVICES AS REQUIRED IN ACCORDANCE WITH LOCAL AUTHORITY REQUIREMENTS AND A.S. CODES FOR THAT TRADE U.N.O

PROTECTION OF WORKS AS REQUIRED BY LOCAL AUTHORITIES

CLEANING OF SITE TO EPA REQUIREMENTS BY CONTRACTOR

ASBESTOS

CONTRACTOR TO ALLOW FOR COMPLETE DEMOLITION & REMOVAL OF ALL ASBESTOS PRODUCTS. ALL ASBESTOS PRODUCTS TO BE REMOVED & HANDLED AS PER RELEVANT AUSTRALIAN STANDARDS

EXTENT OF DEMOLITION WORK AS INDICATED ON THIS DRAWING & HERITAGE CONSULTANTS DRAWINGS & SPECIFICATIONS, INCLUDING BUT NOT LIMITED TO THE FOLLOWING:

REMOVAL/DEMOLITION OF ALL EXISTING FLOORS/SLABS & FOOTINGS, EXTERNAL AND INTERNAL WALLS, WINDOWS, DOORS, CLADDING, GUTTERS, DOWNPIPES, RAINWATER HEADS, CAPPINGS, FLASHING GUTTER BOARD SUPPORTS ETC. U.N.O

REMOVAL/DEMOLITION OF ALL EXISTING WORKS INCLUDING BUT NOT LIMITED TO EXTERNAL STEPS, PATHS, CARPARKS, PLANTERS & LANDSCAPING, GATES, FENCES, GARDEN WALLS, RETAINING WALLS, BOLLARDS, LIGHT BLADES SIGNS, COLUMNS ETC. U.N.O

REMOVAL/DEMOLITION OF ALL EXISTING FIXTURES AND FITTINGS INCLUDING BUT NOT LIMITED TO LIGHTS, TAPS, BASINS, WCS ETC. DISCONNECTION & REMOVAL OF EXISTING SERVICES TO BE UNDERTAKEN BY APPROPRIATE CONTRACTORS

CONTRACTOR TO MAKE GOOD DAMAGE TO ADJOINING BUILDINGS, PAVEMENT, LANEWAYS, ROADS, FENCES, KERBS, CHANNELS, SERVICES, STREET FURNITURE, CARSPACE MARKINGS ETC. (IE. ANY ADJOINING OBJECTS/MATERIALS NOT WITHIN SITE BOUNDARY) RESULTING FROM DEMOLITION WORKS.

ALL REPAIRS TO AUTHORITY REQUIREMENTS OR TO CONDITION PRIOR TO DEMOLITION. CONTRACTORS ARE RESPONSIBLE FOR RELOCATION AND CO-ORDINATION OF ROAD CARPARKING MARKINGS, TICKET MACHINES ETC.

DUST CONTROL

THE TECHNIQUES ADOPTED FOR STRIPPING OUT AND FOR DEMOLITION SHALL MINIMISE THE RELEASE OF DUST INTO THE ATMOSPHERE. BEFORE THE COMMENCEMENT OF STRIPPING OR DEMOLITION IN AN AREA OF A STRUCTURE, ANY EXISTING ACCUMULATIONS OF DUST IN THAT AREA SHALL BE COLLECTED, PLACED IN SUITABLE CONTAINERS AND REMOVED. SELECTION OF AN APPROPRIATE COLLECTION TECHNIQUE, SUCH AS VACUUMING OR HOSEING DOWN, SHALL TAKE DUE ACCOUNT OF THE NATURE OF THE DUST AND THE TYPE OF HAZARD IT PRESENTS. DUST GENERATED DURING STRIPPING, OR DURING THE BREAKING DOWN OF THE BUILDING FABRIC TO REMOVABLE SIZED PIECES, SHALL BE KEPT DAMP UNTIL IT IS REMOVED FROM THE SITE OR CAN BE OTHERWISE CONTAINED. THE USE OF EXCESS WATER FOR THIS PURPOSED SHALL BE AVOIDED.

NOISE CONTROL

NOISE SHALL BE MINIMISED AS FAR AS PRACTICABLE, BY THE SELECTION OF APPROPRIATE METHODS AND EQUIPMENT, AND BY THE USE OF SILENCING DEVICES WHEREVER PRACTICABLE TO EPA/CODE REQUIREMENTS.

NOTE:

1. ATTENTION IS DRAWN TO RECOMMENDATIONS IN AS.2436

2. HOURS OF OPERATING EQUIPMENT MAY BE RESTRICTED BY REGULATORY AUTHORITY

FIRE SERVICES

WHERE A FIRE HYDRANT SERVICE OR A FIRE HOSE REEL SERVICE IS PROVIDED IN A BUILDING, IT SHALL BE AVAILABLE AT ALL TIMES DURING THE DEMOLITION OF THE BUILDING, SO THAT ALL REMAINING STOREYS EXCEPT THE TWO UPPERMOST STOREYS, ARE SERVICED. ACCESS TO THE FIRE PROTECTION SERVICES, INCLUDING ANY BOOSTER FITTING, SHALL BE MAINTAINED AT ALL TIMES. IF A SPRINKLER SYSTEM IS INSTALLED IN A BUILDING TO BE DEMOLISHED, IT SHALL BE MAINTAINED IN AN OPERABLE CONDITION AT EACH STOREY BELOW THE TWO UPPERMOST UNSTRIPPED STOREYS.

SUITABLE PORTABLE FIRE EXTINGUISHERS SHALL BE KEPT AT ALL TIMES IN WORKING AREA AND ARE NOT PROTECTED BY OTHER FIRE SERVICES, AND MAINTAINED IN AN OPERABLE CONDITION.

PRELIMINARY

PRECAUTIONARY MEASURES TO BE UNDERTAKEN AS DESCRIBED IN AS.2601-2001. A "HAZARDOUS SUBSTANCES MANAGEMENT PLAN" IS TO BE IMPLEMENTED BEFORE AND DURING DEMOLITION. IF THE PRELIMINARY INVESTIGATION OF THE BUILDING IDENTIFIES ANY HAZARDOUS MATERIALS CONTAINED THEREIN, THESE MATERIALS WILL BE REMOVED AS PER THE GUIDELINES OF THE RESPONSIBLE AUTHORITY AND CLEARANCE CERTIFICATE OBTAINED BEFORE DEMOLITION COMMENCES

SAFETY FENCING

SECURITY FENCES SHALL BE PROVIDED TO THE STREET BOUNDARY OF THE DEMOLITION SITE AND ANY ADDITIONAL PRECAUTIONARY MEASURES, TAKEN AS MAY BE NECESSARY TO PREVENT UNAUTHORISED ENTRY TO THE SITE, WHERE THE DEMOLITION SITE ADJOINS A PUBLIC THOROUGHFARE THE COMMON BOUNDARY BETWEEN THEM SHALL BE FENCED FOR ITS FULL LENGTH WITH A HOARDING UNLESS THE LEAST HORIZONTAL DISTANCE BETWEEN THE COMMON BOUNDARY AND THE NEAREST PART OF THE STRUCTURE IS GREATER THAN TWICE THE HEIGHT OF THE STRUCTURE. THE FENCING SHALL BE THE EQUIVALENT OF THE CHAIN WIRE AS SPECIFIED IN AS.1725

NOTICES LETTERED IN ACCORDANCE WITH AS.1319 AND DISPLAYING THE WORDS "WARNING DEMOLITION IN PROGRESS", OR SIMILAR, SHALL BE FIXED TO THE FENCING AT APPROPRIATE PLACES TO WARN THE PUBLIC. PROVISION SHALL BE MADE FOR READY ACCESS TO THE SITE BY EMERGENCY SERVICES PERSONNEL IN THE EVENT OF FIRE OR ACCIDENT

DEMOLITION WORK

STRUCTURES SHALL BE DEMOLISHED IN THE REVERSE ORDER TO THAT OF THEIR CONSTRUCTION. THE ORDER OF DEMOLITION FOR BUILDINGS SHALL BE PROGRESSIVE, STOREY BY STOREY, HAVING PROPER REGARD TO THE TYPE OF CONSTRUCTION, NO PART OF ANY STRUCTURE SHALL BE LEFT UNSUPPORTED OR UNATTENDED IN SUCH A CONDITION THAT IT MAY COLLAPSE OR BECOME DANGEROUS. PRECAUTIONS SHALL BE TAKEN TO ENSURE THAT THE STABILITY OF ALL PARTS OF THE STRUCTURE, AND THE SAFETY OF PERSONS ON AND OUTSIDE THE SITE, WILL BE MAINTAINED IN THE EVENT OF SUDDEN AND SEVERE WEATHER CHANGES.

WALLS ON THE BOUNDARIES ARE TO BE DEMOLISHED IN A SAFE AND WORKMANSHIP LIKE MANNER. WALLS SHALL NOT BE LATEROALLY LOADED BY ACCUMULATED DEBRIS OR RUBBLE, TO THE EXTENT THAT THEY ARE IN DANGER OF COLLAPSE. ALLOWABLE LOADING OF THE SUSPENDED FLOORS SHALL BE DETERMINED BY AN INDEPENDENT STRUCTURAL ENGINEER.

ADJOINING PROPERTIES

SAFE ACCESS TO AND EGRESS FROM ADJOINING BUILDINGS SHALL BE MAINTAINED AT ALL TIMES FOR THE DURATION OF THE DEMOLITION WORK. NO DEMOLITION ACTIVITY SHALL CAUSE DAMAGE TO OR ADVERSELY AFFECT THE STRUCTURAL INTEGRITY OF ADJOINING BUILDINGS. THE EFFECTS OF VIBRATION AND CONVERSION ON ADJOINING BUILDINGS AND THEIR OCCUPANTS SHALL BE MINIMISED BY SELECTING DEMOLITION METHODS AND EQUIPMENT APPROPRIATE TO THE CIRCUMSTANCES. WHERE ANY SURFACE OF AN ADJOINING BUILDING IS EXPOSED BY DEMOLITION, THE NEED FOR WEATHERPROOFING THE EXPOSED SURFACE SHALL BE INVESTIGATED AND TEMPORARY OR PERMANENT PROTECTION PROVIDED AS APPROPRIATE. PRECAUTIONS SHALL BE TAKEN TO MINIMISE THE SPREADING OF MUD AND DEBRIS BY VEHICLES LEAVING THE SITE.

GENERAL

OPENINGS IN EXISTING WALLS AND FLOORS, THROUGH WHICH THERE IS A RISK OF PERSONS FALLING TO LOWER LEVEL, SHALL BE EITHER PROPERLY GUARDED OR BOARDED OVER AND THE BOARDING SECURED AGAINST UNAUTHORISED OR ACCIDENTAL REMOVAL. PRECAUTIONS SHALL BE TAKEN TO PREVENT GLASS FROM FALLING ON ANY PERSON IN OR OUTSIDE THE BUILDING. DEMOLISHED MATERIAL SHALL NOT BE ALLOWED TO FALL FREELY OUTSIDE THE STRUCTURE UNLESS IT IS CONFINED WITHIN A CHUTE OR SIMILAR ENCLOSURE, WHICH IS CLEAR OF OBSTRUCTIONS TO OBJECTS FALLING FREELY.

DEMOLISHED MATERIAL SHALL BE REMOVED PROGRESSIVELY FROM THE SITE AND AT ANY TIME, SHALL NOT BE ALLOWED TO ACCUMULATE TO THE EXTENT THAT IT PRESENTS A HAZARD TO THE PUBLIC OR TO SITE PERSONNEL.

ALL DRAWINGS TO BE READ IN CONJUNCTION WITH HERITAGE CONSULTANTS DRAWINGS AND SPECIFICATIONS

SITE PROTECTION/SHORING LEGEND

PERMANENT SITE SHORING

TEMPORARY SITE SHORING

NOTE:

REFER STRUCTURAL ENGINEERS DRAWINGS FOR ALL SITE PROTECTION AND SHORING DETAILS

LEGEND

- BULK EXCAVATION
- STRUCTURES TO BE DEMOLISHED
- TREES TO BE REMOVED
- SEDIMENT CONTROL FENCE
- SECURITY FENCE

VOTRAINT NO124 PTY LTD
PROJECT NO: 2201



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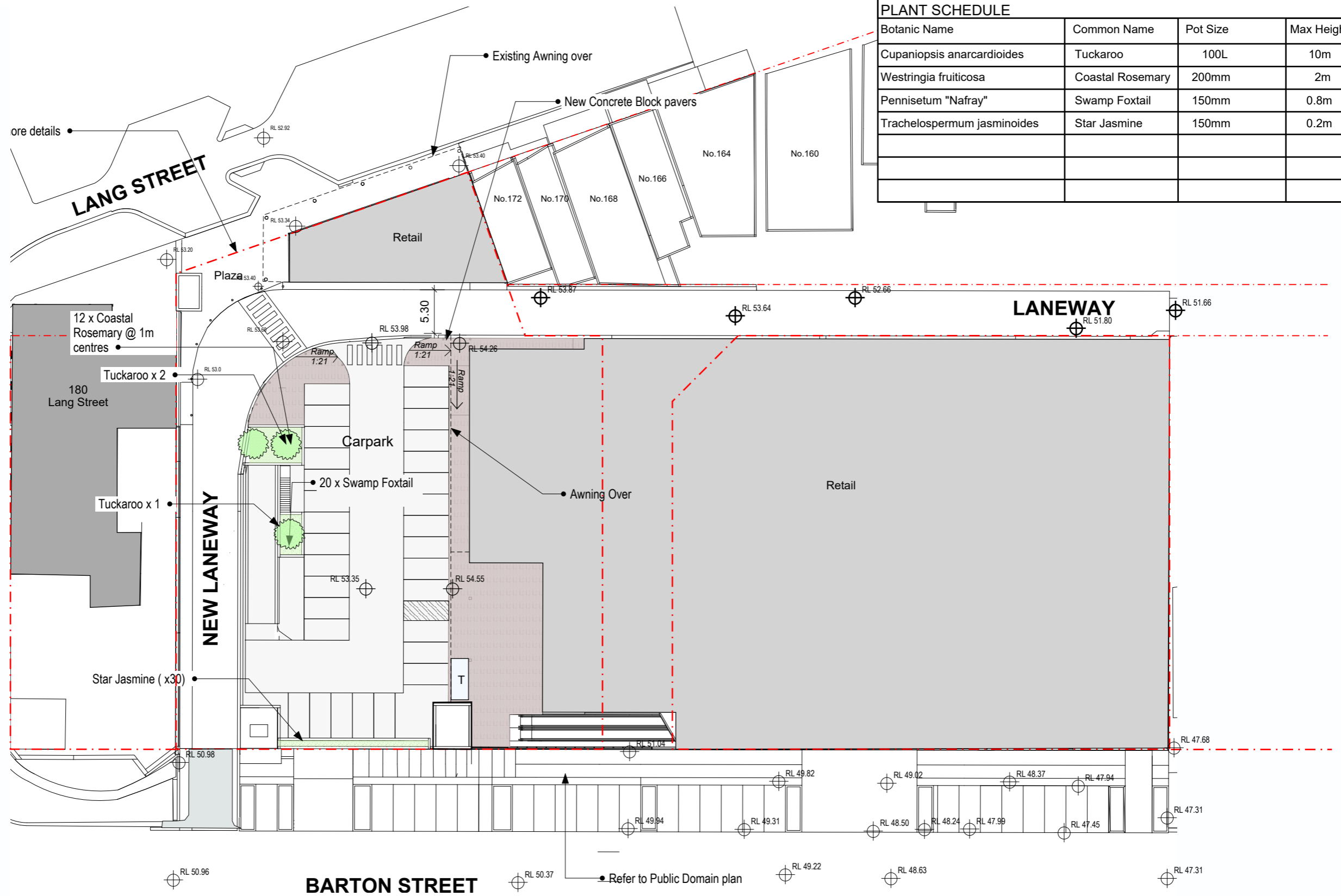
PROJECT TITLE: 174-178 LANG STREET, KURRI KURRI
ISSUED FOR DEVELOPMENT APPLICATION

Drawing Title
Drawing Scale
Issue Date
Revision

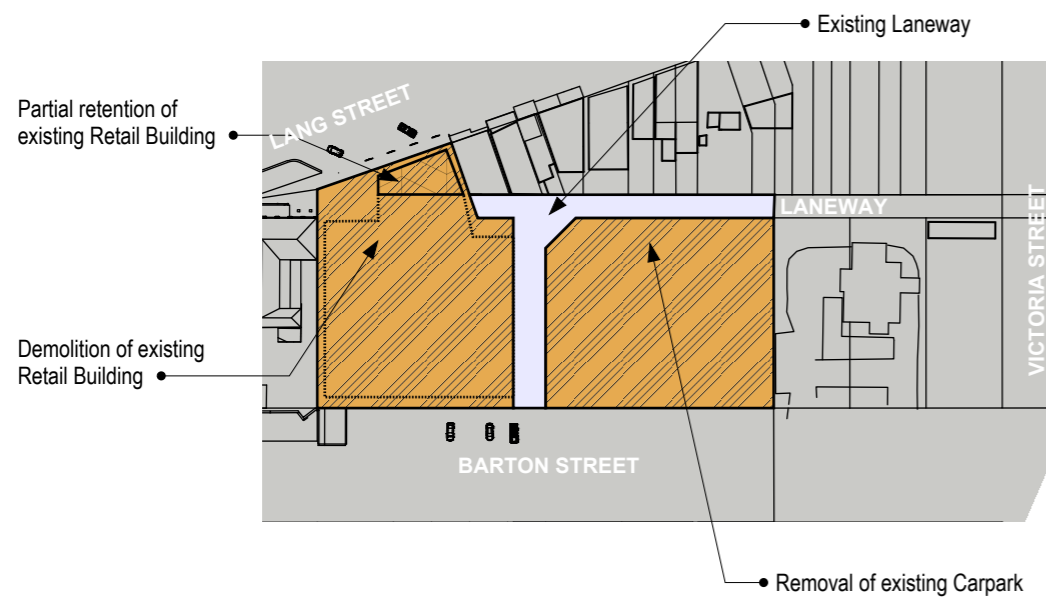
DEMOLITION PLAN
1:800@A3
21/08/2023
B

Drawing
DA04

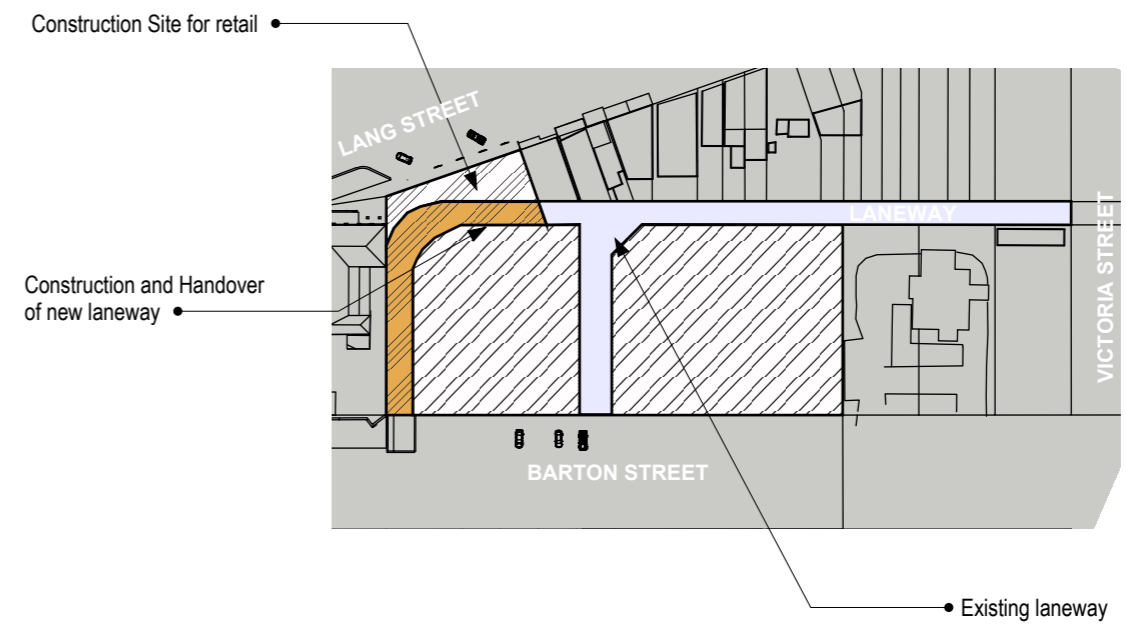
HAMPDEN STREET



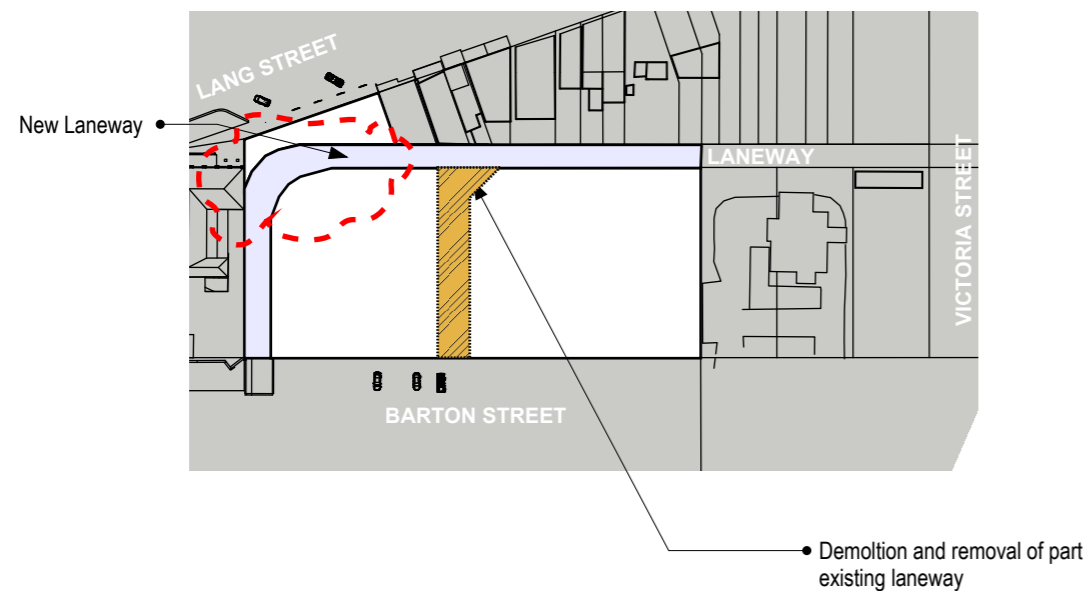
PLANT SCHEDULE				
Botanic Name	Common Name	Pot Size	Max Height	No.
Cupaniopsis anarcardioides	Tuckaroo	100L	10m	3
Westringia fruticosa	Coastal Rosemary	200mm	2m	12
Pennisetum "Nafray"	Swamp Foxtail	150mm	0.8m	20
Trachelospermum jasminoides	Star Jasmine	150mm	0.2m	30



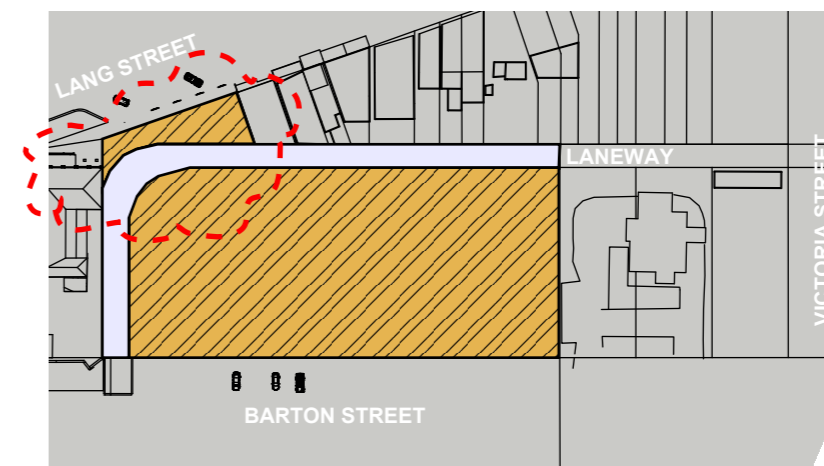
Stage 1 - Demolition




Stage 2a - Construction of laneway and Handover



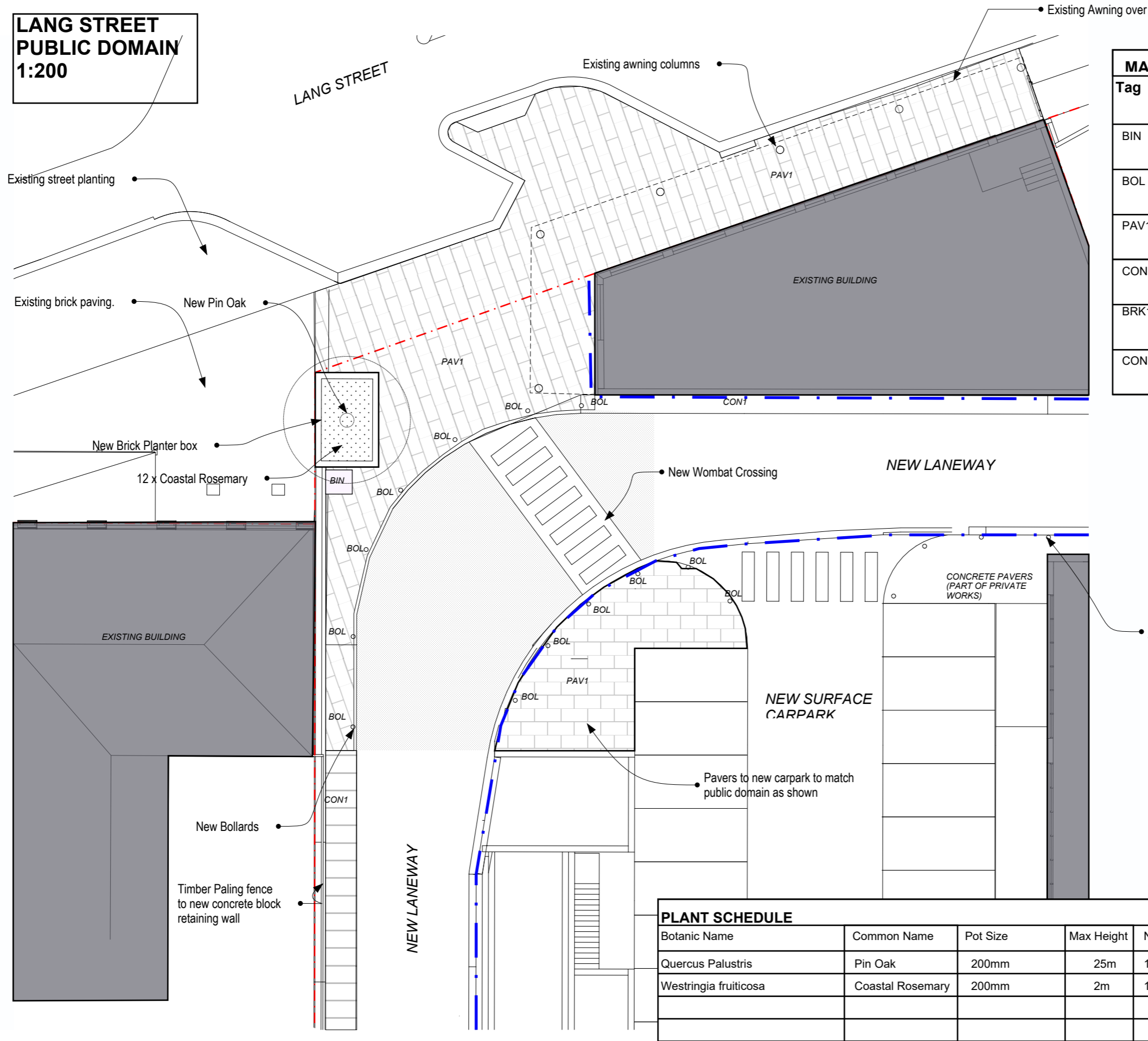
Stage 2b - Closure of Existing Lane and handover



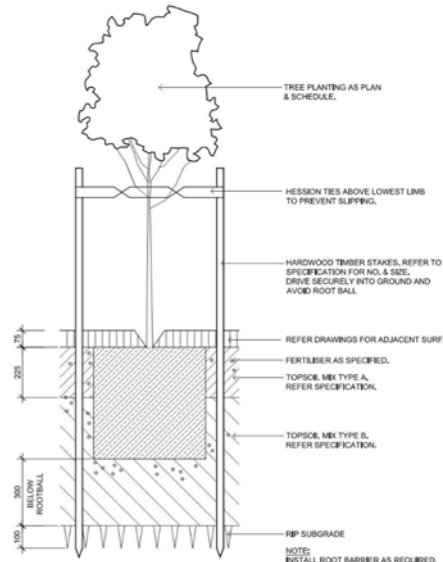
Stage 3 - Construction

VOTRAINT NO124 PTY LTD PROJECT NO: 2201	 NORTH	This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Finished ground levels on the plan are subject to site conditions. Do not scale from drawings. Figured dimensions are to be given preference over scale.	PROJECT TITLE: 174-178 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title Drawing Scale Issue Date Revision	SITE STAGING PLAN 1:1000@A3 21/08/2023 B	Drawing DA06
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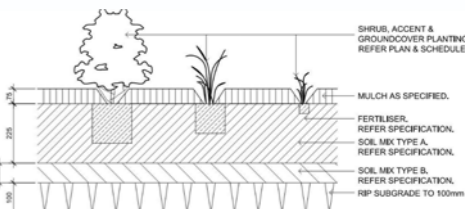
LANG STREET
PUBLIC DOMAIN
1:200



MATERIALS AND FIXTURES SCHEDULE				
Tag	Item	Name	Code	Supplier
BIN	Bin Enclosure: Commercial Centre and Carparks	Manhattan Bin	LR6533	Commercial Systems Australia
BOL	Bollard: Commercial Centre and Carparks	Urbania Bollard	SB2101	Commercial Systems Australia
PAV1	Paving: Bluestone	Natural	Sand	Sam the Paving man
CON1	Paving: Conc Footpath	Warm Grey	Broom	N/A
BRK1	Brick: 600mm high brick planter box	Everyday Life:	SKU 2302500126200 00301150	Austral Bricks
CON1	Paving: Conc Footpath	Warm Grey	Broom	N/A



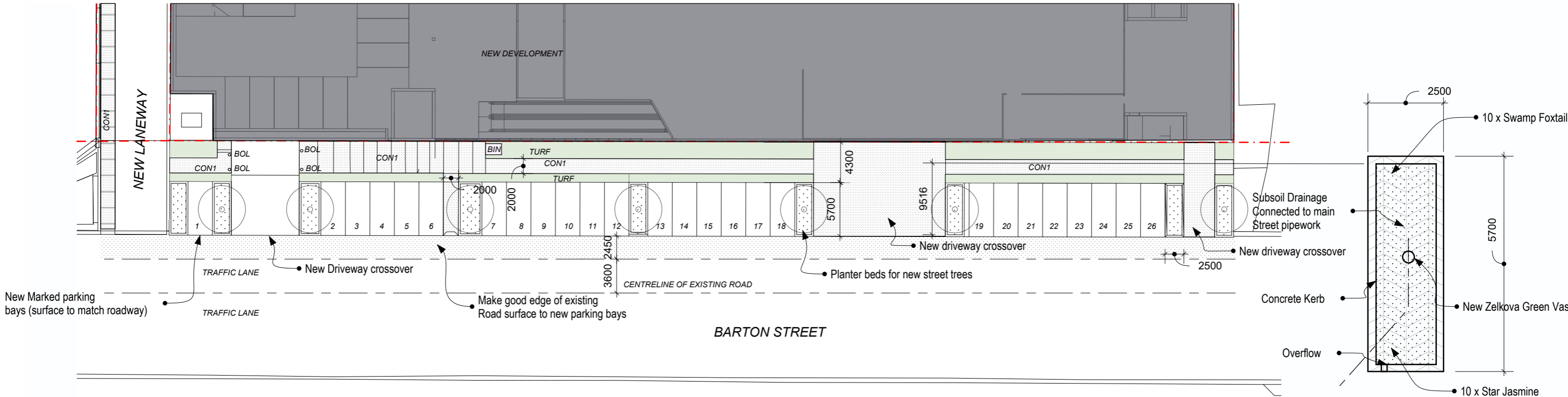
75-200L TREE
PLANTING



SHRUB AND ACCESS
PLANTING

PLANT SCHEDULE				
Botanic Name	Common Name	Pot Size	Max Height	No.
Quercus Palustris	Pin Oak	200mm	25m	1
Westringia fruticosa	Coastal Rosemary	200mm	2m	12





BARTON STREET
PUBLIC DOMAIN
1:400

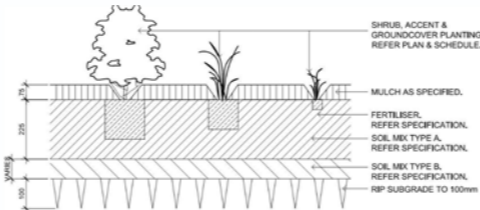
TYPICAL
PLANTING BED
(1:100)

FITTINGS AND FIXTURES SCHEDULE				
Tag	Item	Name	Code	Supplier
BIN	Bin Enclosure: Commercial Centre and Carparks	Manhattan Bin	LR6533	Commercial Systems Australia
BOL	Bollard: Commercial Centre and Carparks	Urbania Bollard	SB2101	Commercial Systems Australia
PAV1	Paving: Bluestone	Natural	Sand	Sam the Paving man
CON1	Paving: Conc Footpath	Warm Grey	Broom	N/A
BRK1	Brick: 600mm high brick planter box	Everyday Life:	SKU 2302500126200 00301150	Austral Bricks
CON1	Paving: Conc Footpath	Warm Grey	Broom	N/A

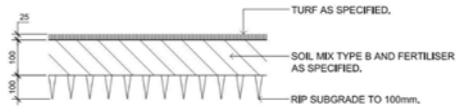
NOTES
Barton street upgrades in accordance with "Barton St East" section of Commercial Centre Masterplan

ZELKOVA
ZELKOVA SERRATA "GREEN VASE"

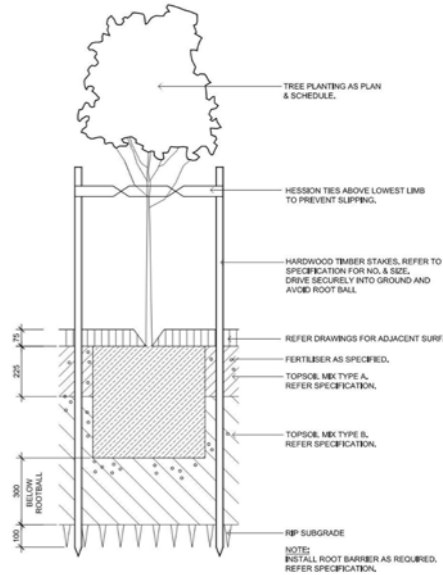
PLANT SCHEDULE				
Botanic Name	Common Name	Pot Size	Max Height	No.
Zelkova Serrata	Green Vase	100L	14m	7
Quercus Palustris	Pin Oak	200mm	25m	1
Westringia fruticosa	Coastal Rosemary	200mm	2m	12
Pennisetum "Nafray"	Swamp Foxtail	150mm	0.8m	50
Trachelospermum jasminoides	Star Jasmine	150mm	0.2m	100



SHRUB AND ACCESS
PLANTING

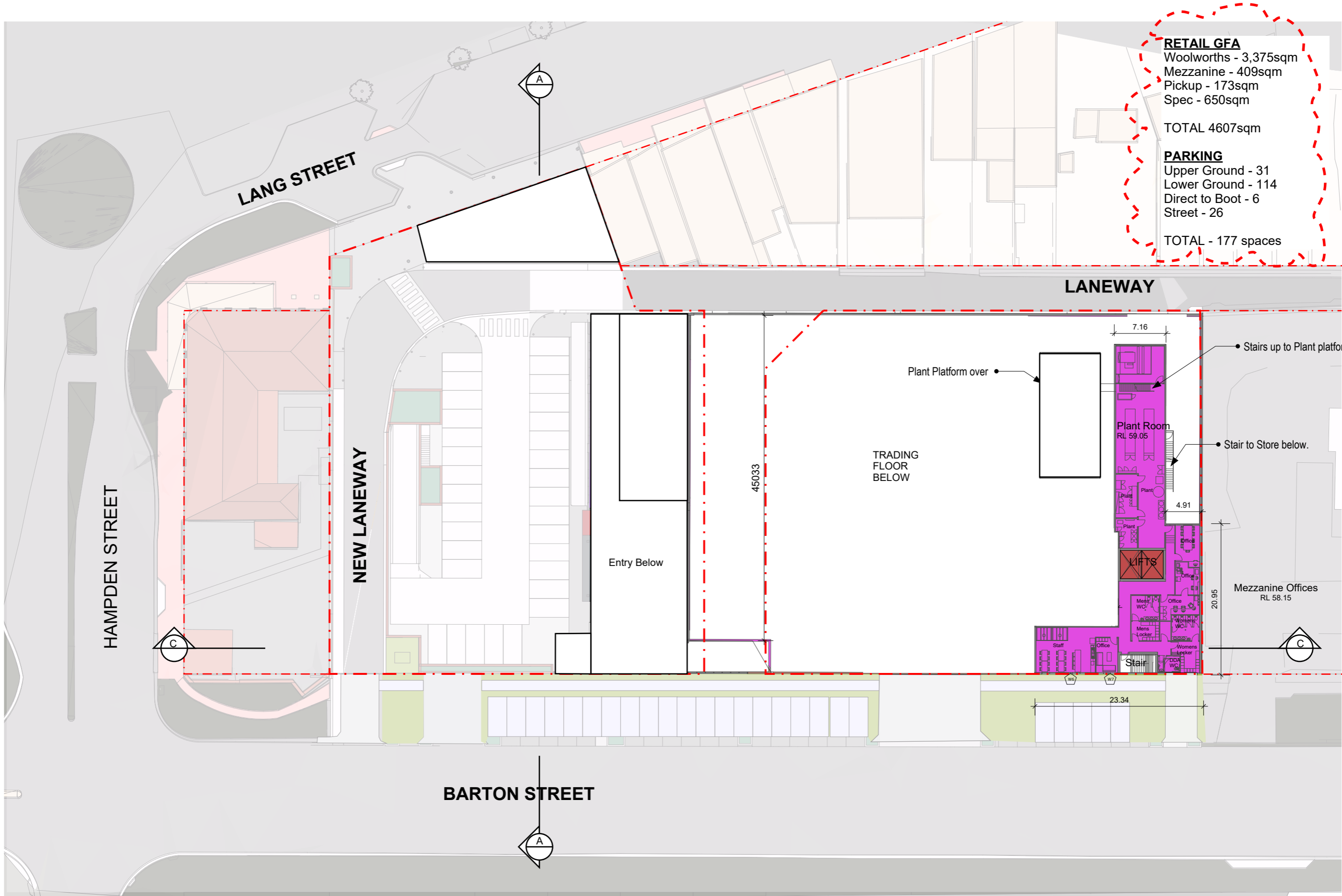


TURF ON EVEN
GRADE



75-200L TREE
PLANTING





RETAIL GFA

Woolworths - 3,375sqm
Mezzanine - 409sqm
Pickup - 173sqm
Spec - 650sqm

TOTAL 4607sqm

PARKING

Upper Ground - 31
Lower Ground - 114
Direct to Boot - 6
Street - 26

TOTAL - 177 spaces

VOTRAINT NO124 PTY LTD
PROJECT NO: 2201



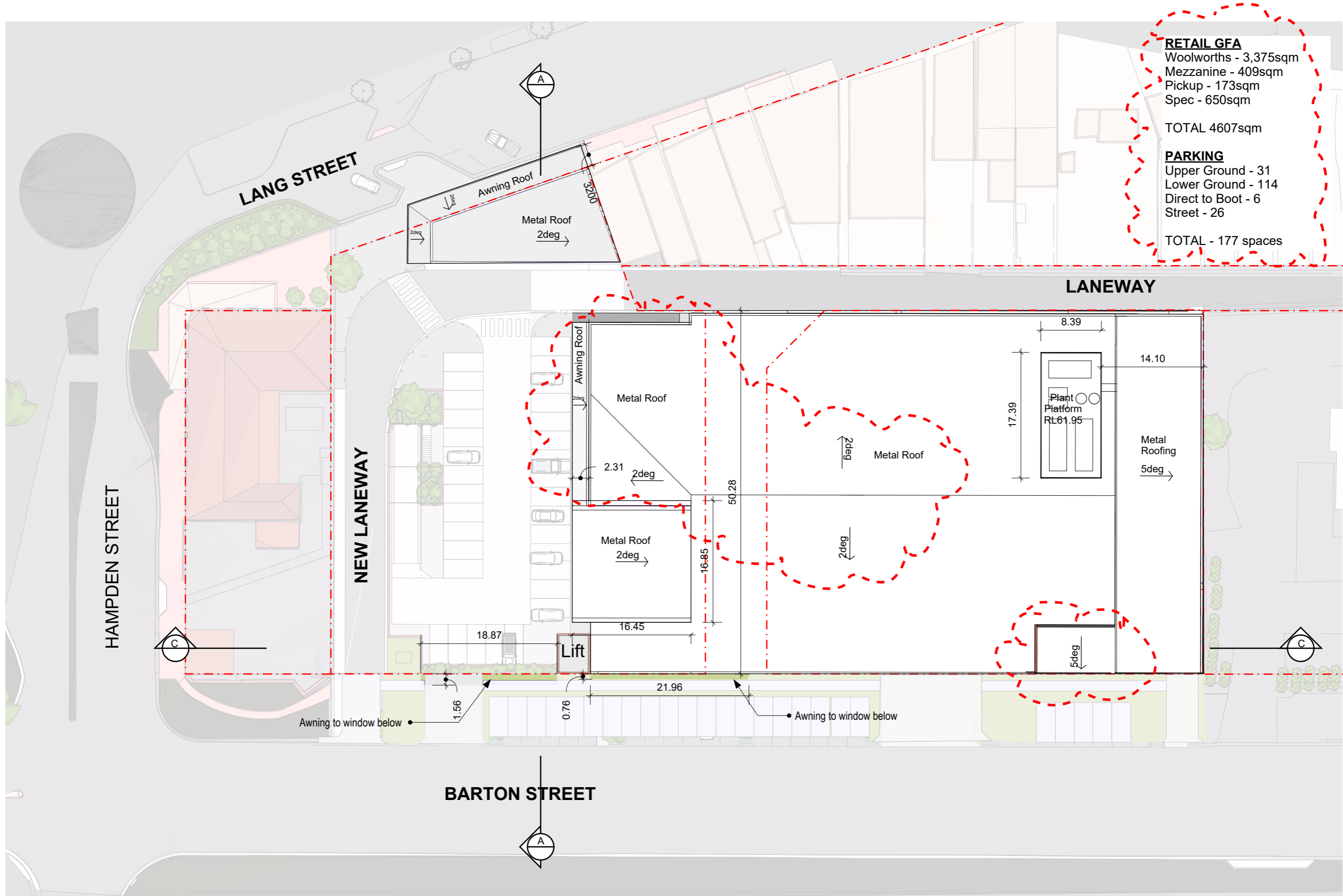
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PROJECT TITLE:
174-178 LANG STREET, KURRI KURRI
ISSUED FOR
DEVELOPMENT APPLICATION

Drawing Title
Drawing Scale
Issue Date
Revision

MEZZANINE FLOOR
1:500@A3
21/08/2023
B

Drawing
DA12



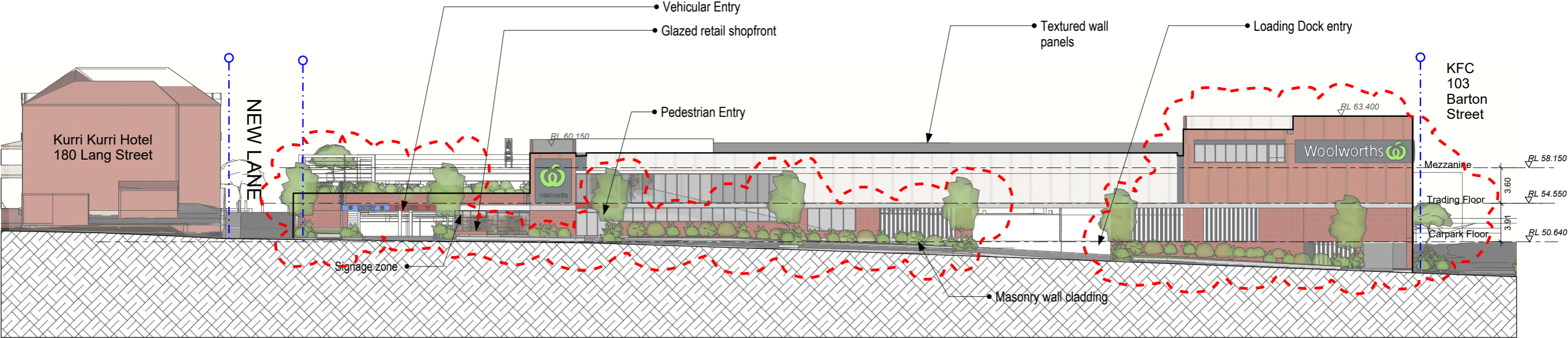
RETAIL GFA
Woolworths - 3,375sqm
Mezzanine - 409sqm
Pickup - 173sqm
Spec - 650sqm

TOTAL 4607sqm

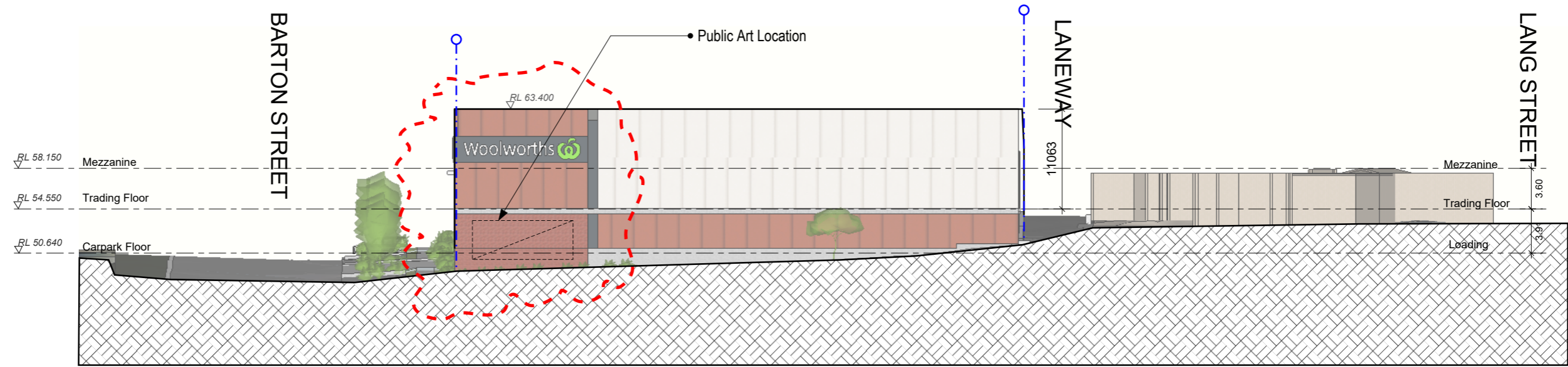
PARKING
Upper Ground - 31
Lower Ground - 114
Direct to Boot - 6
Street - 26

TOTAL - 177 spaces





SOUTH ELEVATION (BARTON STREET)



EAST ELEVATION

MATERIALS LEGEND

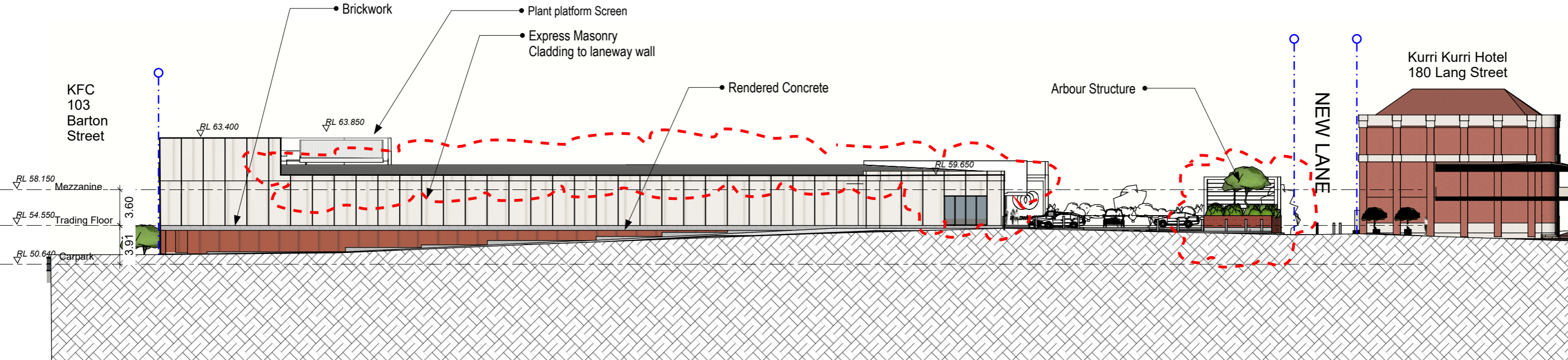
BRK1 - Brickwork	PT1 - White Painted Finish
CON1 - Masonry Wall Panels	GL1 - Glass Panels
CON2 - Concrete Textured	PD1 - Powdercoated Window Frames
CON3 - Concrete Blockwork	PD2 - Powdercoated Screens

VOTRAINT NO124 PTY LTD
PROJECT NO: 2201

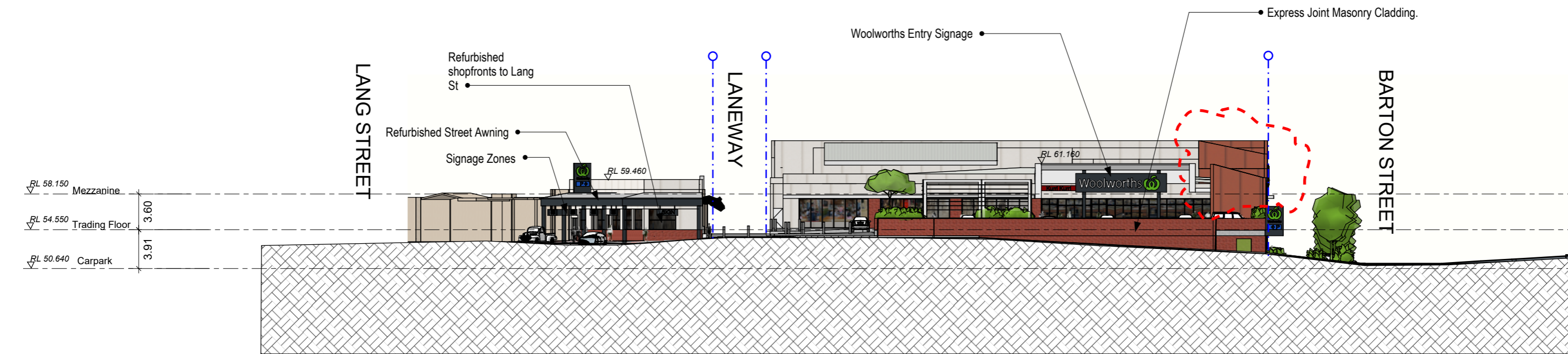
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PROJECT TITLE:	Drawing Title	ELEVATIONS - SOUTH & EAST	Drawing
174-178 LANG STREET, KURRI KURRI	Drawing Scale	1:400@A3	
ISSUED FOR	Issue Date	21/08/2023	
DEVELOPMENT APPLICATION	Revision	B	

DA20



NORTH ELEVATION (LANEWAY)



WEST ELEVATION (NEW LANEWAY)

MATERIALS LEGEND

BRK1 - Brickwork	PT1 - White Painted Finish
CON1 - Masonry Wall Panels	GL1 - Glass Panels
CON2 - Concrete Textured	PD1 - Powdercoated Window Frames
CON3 - Concrete Blockwork	PD2 - Powdercoated Screens

VOTRAINT NO124 PTY LTD
PROJECT NO: 2201

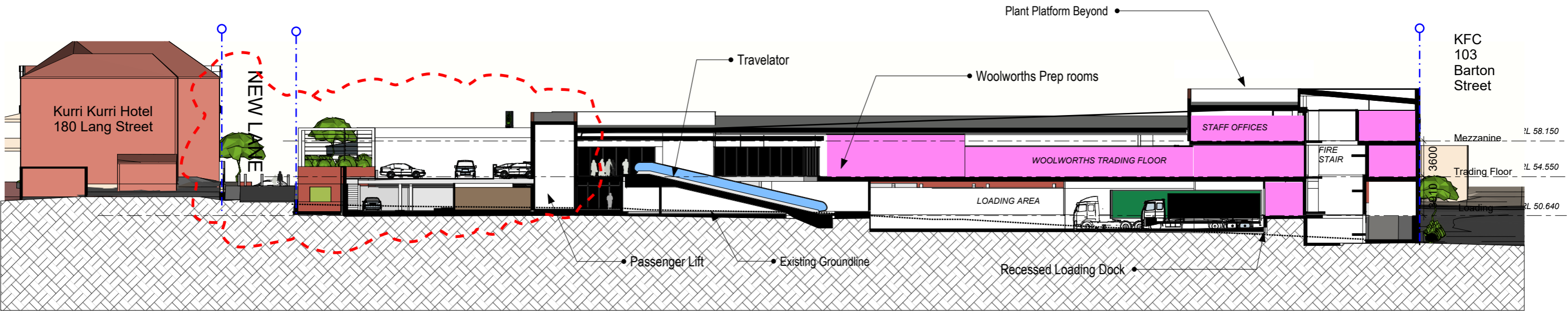
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PROJECT TITLE:
174-178 LANG STREET, KURRI KURRI
ISSUED FOR
DEVELOPMENT APPLICATION

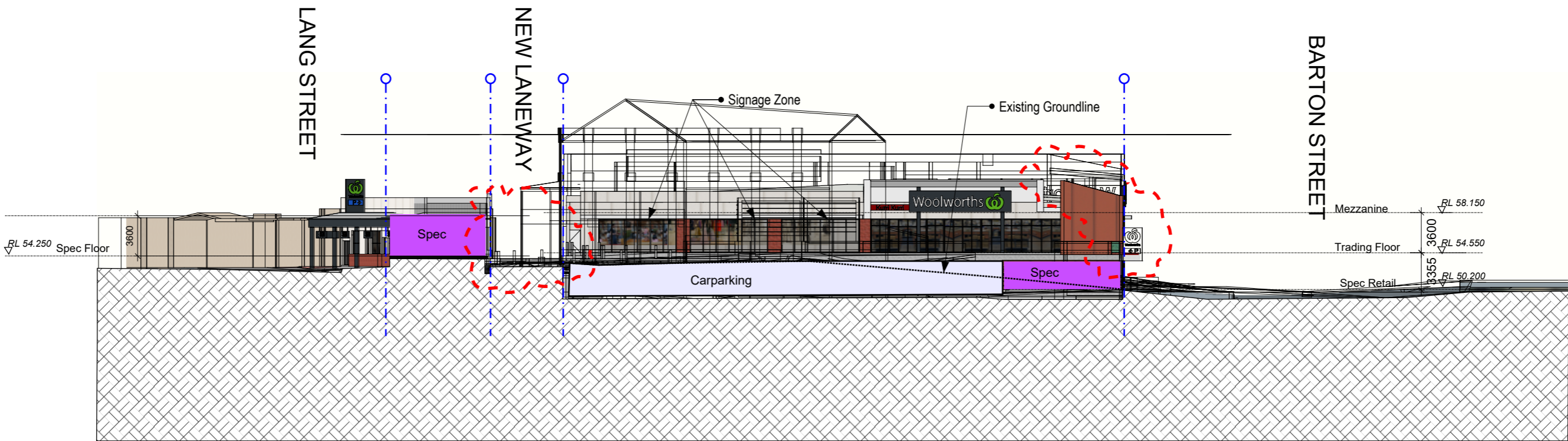
Drawing Title
Drawing Scale
Issue Date
Revision

ELEVATIONS- NORTH & WEST
1:400@A3
21/08/2023
B

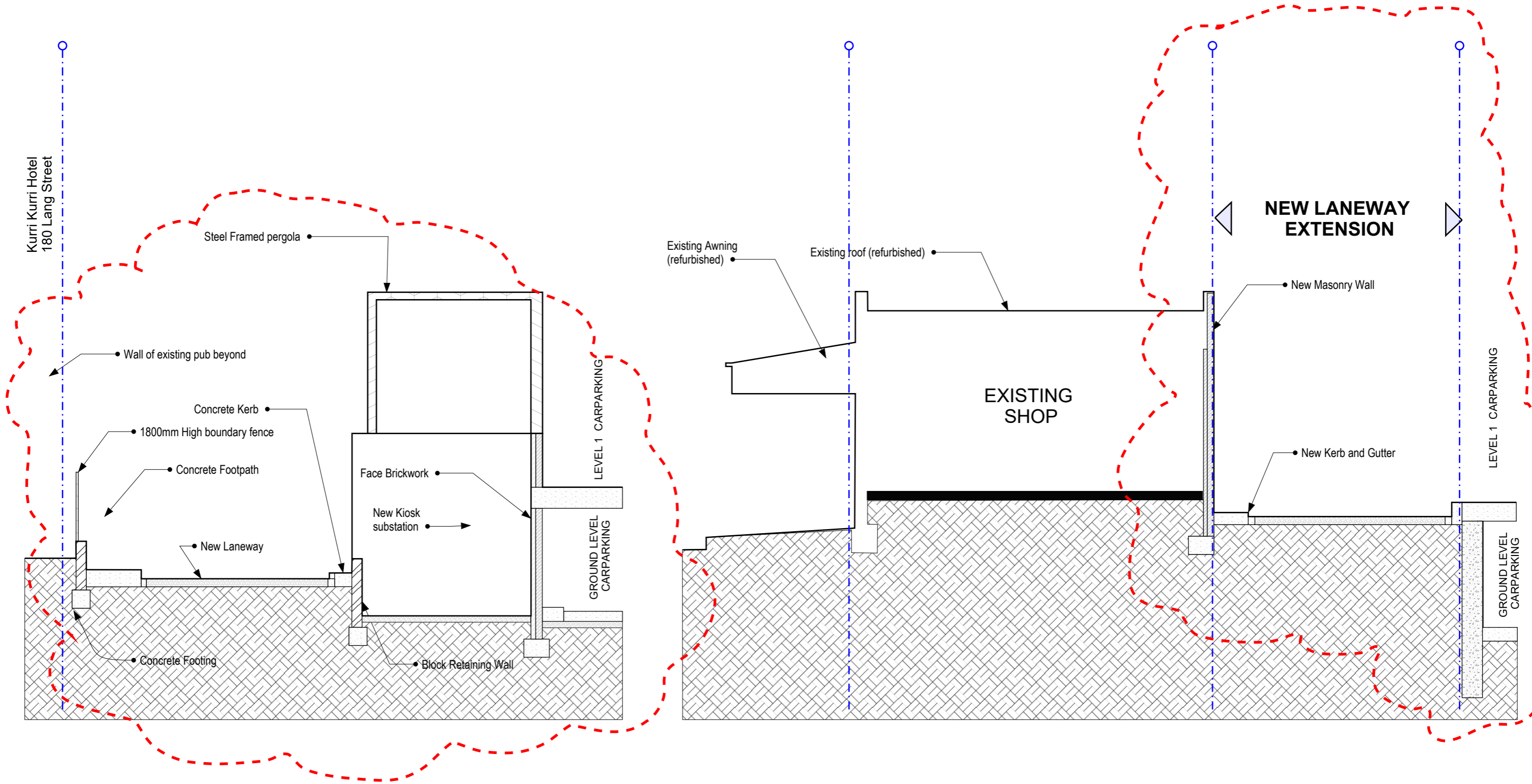
Drawing
DA21



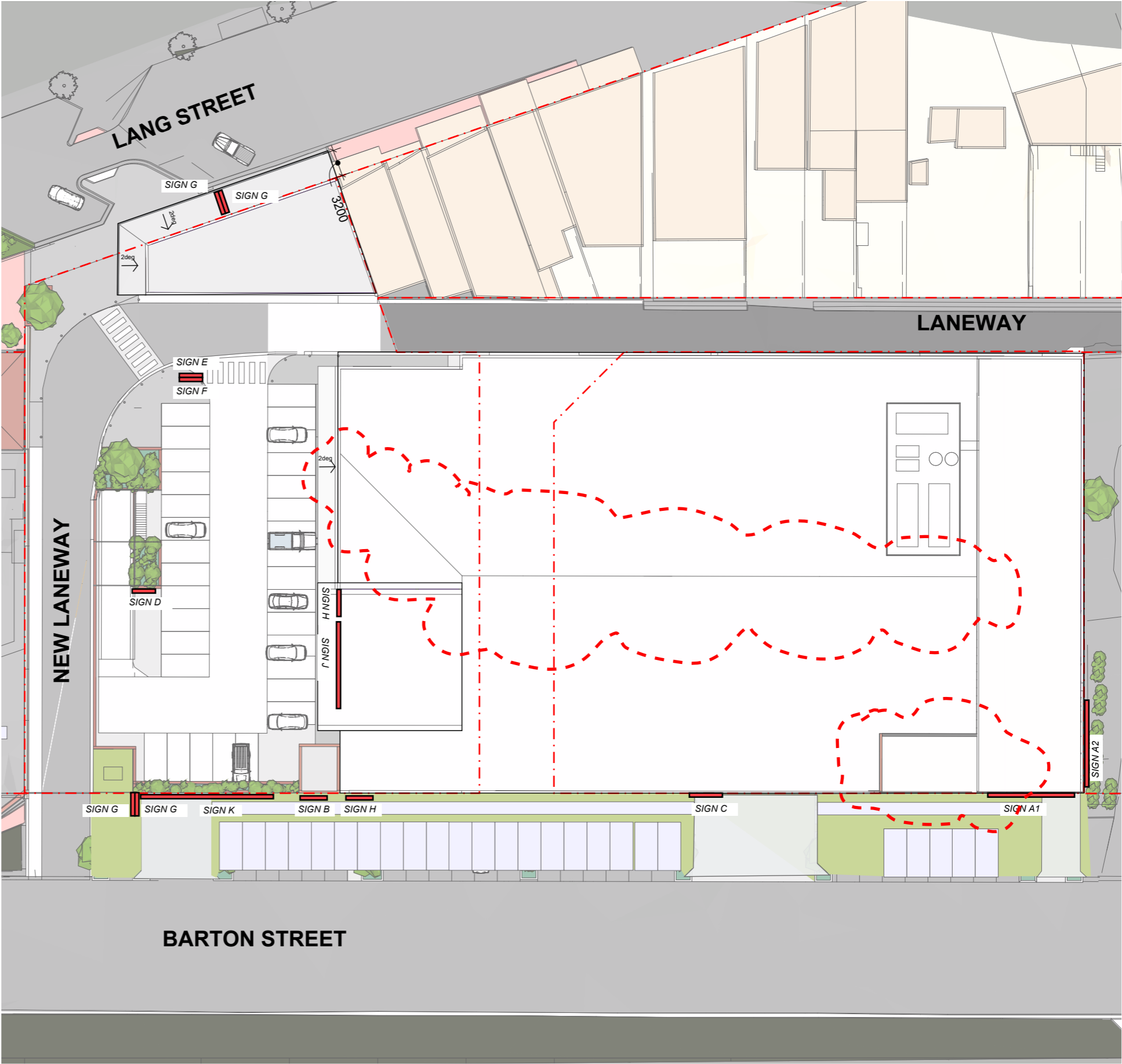
SECTION C-C



SECTION A-A

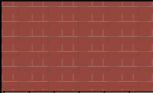

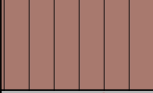

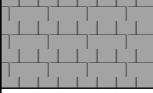




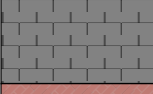



TYPE	GRAPHIC	DESCRIPTION	AREA
SIGN A x 2		Internally illuminated sign	16.5sqm
SIGN B		Internally illuminated Sign	14.4sqm
SIGN C		2d non-illuminated sign	1.2sqm
SIGN D		2d non-illuminated sign	0.6sqm 1.2sqm
SIGN E		Internally illuminated signage with vinyl face	1.44sqm
SIGN F		2d non-illuminated signage	1.44sqm
SIGN G		Internally illuminated signage with vinyl face	5.4sqm
SIGN H		Internally illuminated signage with vinyl face	2.2sqm
SIGN J		Internally illuminated sign	16.5sqm
SIGN K		Internally illuminated signage with vinyl face	4.7sqm

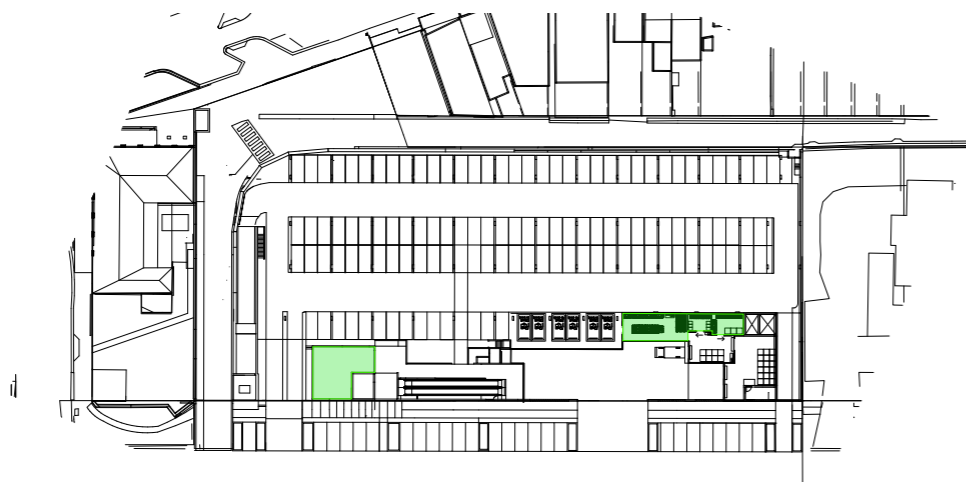




MATERIALS LEGEND

		BRK1 - Brickwork
		CON1 - Masonry Wall Panels Off White
		CON4 - Masonry Wall Panels Oxide
		CON2 - Concrete Textured
		CON3 - Concrete Blockwork
		PT1 - White Painted Finish
		GL1 - Glass Panels
		PD1 - Powdercoated Window Frames
		PD2 - Powdercoated Screens
		PAV1 - Concrete Pavers
		PAV2 - Brick Pavers

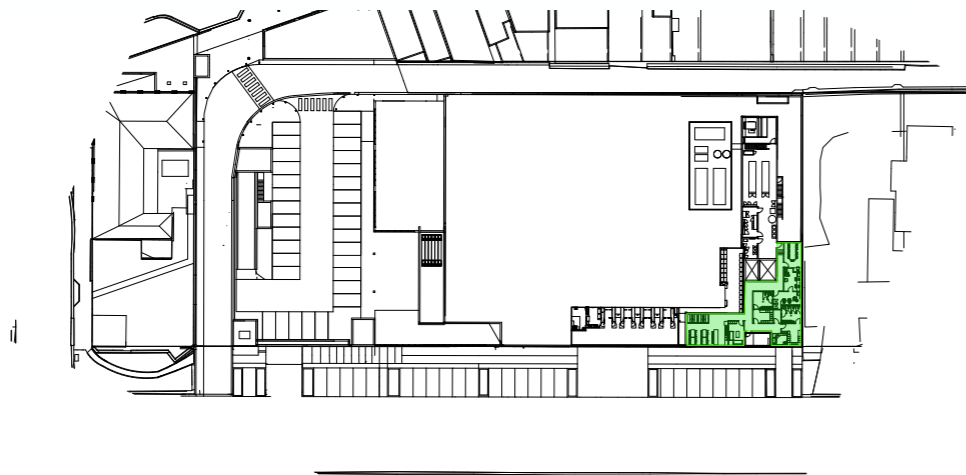
Perspective
View from new on grade parking
towards supermarket entry point.



LOWER GROUND PLAN
1:1500@A3



GROUND PLAN
1:1500@A3



MEZZANINE PLAN
1:1500@A3

GFA CALCULATIONS

Lower Ground - 211.79sqm
 Ground - 3,695.72sqm
 Mezzanine - 231.9sqm

TOTAL - 4,139.41sqm



APPENDIX B - CRIME RISK ASSESSMENT

SITE OPPORTUNITY

Project:

Woolworths Shopping Centre, specialised retail and parking.

174-178 Lang Street, Kurri Kurri

Description:

The following Crime Risk Assessment (CRA) is for the proposed development, which is a new Woolworths Supermarket, three specialised retail premises, offices, loading area, plaza and carparking areas including a basement carpark. Some ratings are assumptions based on the level of documentation provided. The assessment considers whether it is possible to achieve a good or a bad rating not if its necessarily provided at this approval stage.

1	Surveillance	Good	Bad	N/A
1	Buildings – orientation		X	
2	Buildings – frontages/set back		X	
3	Buildings – windows, doors, balconies etc		X	
4	Buildings – lobbies, foyers, lifts etc		X*	
5	Buildings – internal visibility	X		
6	Buildings – loading docks/delivery areas	X		
7	Buildings – communal areas	X**		
8	Grade separated space	X		
9	Spatial gaps/vacant land	X		
10	Public telephones/Help Points	X***		
11	Automatic teller machines			X#
12	Transport shelters/stands	X		
13	Off-street parking		X	
14	Underpasses/tunnels			X
15	Overpasses/footbridges			X
16	Car park – internal obstructions		X	
17	Car park – configuration of bays	X		
18	Car park – ceiling height		X##	
19	Fencing/perimeter visibility	X		
20	Public toilets and change rooms		X	
21	Parks			X
22	Playgrounds			X
23	Pedestrian and cyclist pathways/routes	X		
24	Wayfinding	X		
25	Vegetation – type/quality	X		
26	Vegetation – coverage/quantity	X		
27	Street furniture	X		
28	Bicycle parking			X
29	Concealment/entrapment opportunities		X	
Total		14	9	7

* Rating due to the 24/7 lift access recommended by Council

** Communal area for this development is the plaza.

*** Assumes a help point will be provided in the basement carpark as per the report recommendations.

Unknown

Car parks with above average ceiling heights (>3 metres) can be expensive to light to minimum Australian Standards.

2	Lighting/Technical Supervision	Good	Bad	N/A
30	Lighting – type	X		
31	Lighting – brightness	X		
32	Lighting – distribution/reflection	X		
33	Lighting – colour rendition	X		
34	Lighting – vandal resistance	X		
35	Lighting – obstructions	X		
36	Lighting – of signs and important structures	X		
37	Mirrors – corridors, tunnels, fire exit stairs	X		
38	Mirrors – ATMs and nightsafes	X		
39	CCTV – type/use	X		
40	CCTV – coverage	X		
41	CCTV – vandal resistance	X		
42	Help phones/intercoms/public address	X		
Total		11	0	2

Note: above based on the opportunity to provide good lighting in the new development. Assumes a help phone is installed in the basement carpark and Woolworths lobby.

3	Territorial Reinforcement	Good	Bad	N/A
43	Community guardians	X		
44	Formal guardians	X		
45	Clarity of ownership	X		
46	Placemaking/street art/animation	X*		
47	Space transition	X		
48	Celebrated entries	X		
49	Signage and location markers	X		
50	Vulnerability of night workers/shoppers		X	
51	Street vendors/buskers			X
52	Proximity of high risk groups/locations		X	
53	Area reputation	X	X	
Total		8	3	1

* Assumes a mural strategy is used to reduce malicious damage risk.

4	Environmental Maintenance	Good	Bad	N/A
54	Area image		X*	
55	Vandalism/graffiti		X	
56	Rubbish	X		
57	Urban decay	X***		
58	Lighting maintenance	X		
59	Landscaping maintenance	X		
60	Maintenance, other	X		
61	Robustness of structures/materials	X		
62	Rubbish bins	X		
Total		7	2	0

* Rating due to illegal graffiti in adjoining areas.

** Rating due to lack of graffiti removal in adjoining areas and assumes no change.

*** Improved rating due to new development

5	Activity and Space Management	Good	Bad	N/A
63	Clarity of land use	X		
64	Conflicting activity	X		
65	Safe activities are located in unsafe areas	X		
66	Unsafe activities are located in safe areas	X		
67	Proximity to licensed premises		X	
68	Night activity/transport		X	
69	Street activity during the night		X	
70	Street activity during the day	X		
71	Functional vulnerability/mixed zoning		X	
72	Crime displacement		X	
73	Neighbourhood edges	X		
<i>Total</i>		6	5	0

6	Access Control	Good	Bad	N/A
74	Street type	X		
75	Linking pathways	X		
76	Buildings – number of entry points		X	
77	Buildings – ease of access to side/rear	X		
78	Buildings – fire exits and stairs		X	
79	Buildings – dumpster bays, loading docks		X	
80	Buildings – natural ladders	X		
81	Gardens – storage sheds			X
82	Doors – security/entry control systems	X		
83	Windows – glazing protection	X		
84	Windows and skylights – security hardware	X		
85	Car park – pedestrian access		X*	
86	Car park – vehicle access		X*	
87	Car park – actual and symbolic barriers		X*	
88	Car park – management of space		X*	
89	Car park – recreational use			X
90	Safe routes	X		
91	Child play areas			X
92	Shortcuts/trespassing opportunities		X	
93	Defensive vegetation	X		
94	Cash carrying routes			X**
95	Reception/cashier/mail rooms	X		
<i>Total</i>		10	8	4

* Rating as the carpark is not currently capable of being secured.

** Further information required for assessment.

7	Design, Definition and Designation of Site	Good	Bad	N/A
96	The design, purpose and definition of the space are in harmony		X	
97	It is clear who is responsible for the space	X		
98	Spatial boundaries/borders reinforce intended function	X	X	
99	Social/cultural norms accord with intended function	X		
100	Legal and administrative requirements are reinforced	X		
<i>Total</i>		4	1	0

PART III: CRIME RISK ASSESSMENT

Table 1.1 – Sub Totals

		Good	Bad	N/A
1	Surveillance	14	9	7
2	Lighting/Technical Supervision	11	0	2
3	Territorial Reinforcement	8	3	1
4	Environmental Maintenance	7	2	0
5	Activity and Space Management	6	5	0
6	Access Control	10	8	4
7	Design/Definition/Designation	4	1	0
	Sub Totals	60	28	14

Table 1.2 – Percentage Rating

Total Applicable Questions = Y (88)

Good	60	÷	Y	x	100	=	68.1%
Bad	28	÷	Y	x	100	=	31.8%

Table 1.3 – ‘Good’ Rating

Total Number of ‘Good’ features/conditions	Risk Rating
0 – 50%	High
51 – 79%	Medium
80 – 100%	Low

Table 1.4 – ‘Bad’ Rating

Total Number of ‘Bad’ features/conditions	Risk Rating
0 – 50%	Low
51 – 79%	Medium
80 – 100%	High

Table 1.5 – CPTED Rating

‘Good’ Rating → ‘Bad’ Rating ↓	Low	Medium	High
Low	LOW	MEDIUM	MEDIUM
Medium	MEDIUM	MEDIUM	HIGH
High	MEDIUM	HIGH	HIGH

Overall Project Crime Risk Rating =

MEDIUM

APPENDIX C - KINGSWAY PLAZA CONSULTATION REPORT

Kingsway Plaza Redevelopment

Consultation Outcomes Report

**174–178 Lang Street
Kurri Kurri**

31 August 2023



ACKNOWLEDGEMENT OF COUNTRY

We are in the Country of the Awabakal and Worimi peoples. We recognise their connection to the land and water of this beautiful and vibrant place. We pay our respects to the Traditional Owners of the land on which we work and pay our respects to Elders past, present and emerging.

Mara team members responsible for this report

Lead	Kelly Lofberg
Senior Consultants	
Mara project number	2302
Report version	Final
	E: mara@maraconsulting.com.au
	W: maraconsulting.com.au

This report has been prepared in accordance with the brief provided by the client and has relied upon the information collected at the time and under the conditions specified in the report. All findings, conclusions or recommendations contained in the report are based on the above circumstances. The report is for the use of the client and no responsibility will be taken for its use by other parties. The client may, at its discretion, use the report to inform internal and external stakeholders.

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COMMONLY USED ACRONYMS AND TERMS

ACRONYMS/TERMS	DESCRIPTION
Council	Cessnock City Council
DA	Development application
DPIE	NSW Department of Planning, Infrastructure and Environment
EIS	Environmental Impact Statement
EP&A Act	Environmental Planning and Assessment Act 1979
FTE	Full-time equivalent
Ha	Hectares
IAIA	International Association for Impact Assessment
HCCPP	Hunter and Central Coast Planning Panel
Km	Kilometres
LEP	Local Environment Plan
LGA	Local Government Area
NSW	New South Wales
SEE	Statement of Environmental Effects

EXECUTIVE SUMMARY

Mara Consulting was engaged by the proponent to conduct community consultation to support the development application for the Kingsway Plaza redevelopment proposal. This report summarises the feedback gathered through the consultation process.

Cessnock City Council (Council) requested as a part of the RFI in May 2021, consultation should be undertaken given the *"the applicant did not undertake consultation prior to lodgement of the development application"*. Subsequently, Mara was commissioned to conduct consultation to meet this request, which occurred between July 2023 and August 2023. The proponent also conducted consultation to support the application, particularly with key stakeholders (tenants, adjacent property owners).

The primary aim of conducting the consultation was two-fold. Establish a dialogue with residents, directly impacted neighbours, and the surrounding community; and secondly, to understand issues and concerns to inform the revision of the development to satisfied Council's request, particularly:

- details of any community engagement undertaken and feedback received
- community engagement methodology and findings
- community engagement took place in accordance with Council's requirements.

Additionally, Council's Social Impact Guidelines state:

"Individuals and groups who could be affected by a proposed development include:

- A. Those who live or work near the development.*
- B. Those who will hear, smell or see the development or its impacts.*
- C. Those who have an interest in or are likely to be affected by the development, but may not live in close proximity (for example: traditional owners, service providers, local committees and community groups).*
- D. Those who may normally use the land/space where the development is to be located."*

To respond to the RFI, and Council's SIA Guidelines, engagement was planned to hear and confirm the issues and concerns from those potentially impacted by the development. This was to make sure all issues were understood by proponent.

Furthermore, the engagement sought to understand concerns, issues and impacts resulting from the proposal and demonstrate how those concerns were addressed in the revised proposal. If concerns could not be addressed, reporting why this was not possible was included. Further details are provided in Table 6 and Table 7.

Stakeholders were included in the consultation plan based on their geographic proximity to the project as well as potential impacts identified as part of the development application (including potential noise, traffic and transport, visual impacts of the proposed development). Stakeholders consulted included approximately 350 landowners, residents, businesses who live up to 300 m of the proposed development, key stakeholders such as government agencies and authorities, Council and the Hunter Central Coast Regional Planning panel, and anyone interested in the proposal.



As of 23 August 2023, more than 650 people had visited the project website seeking information about the project. Additionally, an in-person drop-in session was held with approximately 44 people attending. Interested stakeholders were reached via email, letterbox deliveries, phone calls and door knocking providing project updates and invitations to engagement events. Further detail is included in Table 4 and Table 5.

As such, the engagement and communication activities were designed to satisfy Council's request for consultation with the local community and those immediately affected by the proposed development. Issues and concerns have been identified and considered in the preparation of the revised proposal.



1. Introduction

This consultation outcomes report has been prepared by Mara Consulting (Mara) on behalf of Higgins Planning to provide an overview of the consultation activities carried out to support the resubmission of the development application (DA) reference DA 8/2023/64/1 for the proposed Kingsway Plaza Redevelopment, Kurri Kurri.

The focus of this report is consultation that occurred as a result of the request for information from Cessnock City Council and the Hunter Central Coast Planning Panel (HCCPP). Mara was engaged in June 2023 to conduct community consultation.

Consultation activities post lodgement included:

- proponent-led one-on-one key stakeholder meetings
- establishment of project email address for enquiries and feedback
- letterbox drops to nearby businesses and neighbours
- emails to and meetings with Council and Councillors
- emails to and meetings with key stakeholders
- emails including project information, how to provide feedback and an invitation to attend the drop-in session
- webpage with project information, how to provide feedback, survey link and event details
- drop-in information session
- survey.

This report summarises communication and consultation activities undertaken in relation to the redevelopment proposal. It includes key issues and opportunities raised by the local community, stakeholders, and public authorities as well as a summary of feedback and how it informed the changes to the revised DA submission.

The engagement approach for the project has been guided by the International Association for Public Participation (IAP2) spectrum of public participation, delivered at an 'inform' and 'consult' level. In determining the approach to the communication and consultation, the Department of Planning, Industry and Environment's *Undertaking Engagement Guidelines July 2021*, were considered.

The following report outlines the feedback received.



Overview

The Statement of Environmental Effects (SEE) and accompanying Development Application (DA) submitted to Cessnock City Council (Council) outlines the proposal to redevelop the existing shopping centre located at 174-178 Lang Street, Kurri Kurri. The proposal includes:

- demolition of existing structures and site works
- part road closure, relocation, and extension of existing laneway road with associated subdivision
- alterations and additions to the Lang Street existing pharmacy building
- construction of a new commercial buildings with two specialty shops and new Woolworths supermarket store usage and fit out, including:
 - mezzanine offices, plant room and plant deck
 - car parking
 - "Direct to Boot" bays and storage
 - signage , publicly accessible plaza, and public art space
 - loading facilities
 - landscaping.

During a preliminary assessment meeting with Council on 5 May 2023, Council officers raised a number of areas where more work was required. Subsequently, Council requested as a part of the Request for Information dated 7 May 2023, consultation should be undertaken given the *"applicant did not undertake consultation prior to lodgement of the development application and therefore the Social Impact Assessment does not meet Council Guidelines"*. Council requested:

- targeted consultation with all affected property owners
- adequate community consultation, including those affected by road closure/opening during construction.

While there is no prescriptive standard in Council's policies regarding the level of consultation, Mara has used the *"Undertaking Engagement: Guidelines for State Significant Projects (October 2022)"* (Guideline) as the framework for the approach to consultation.

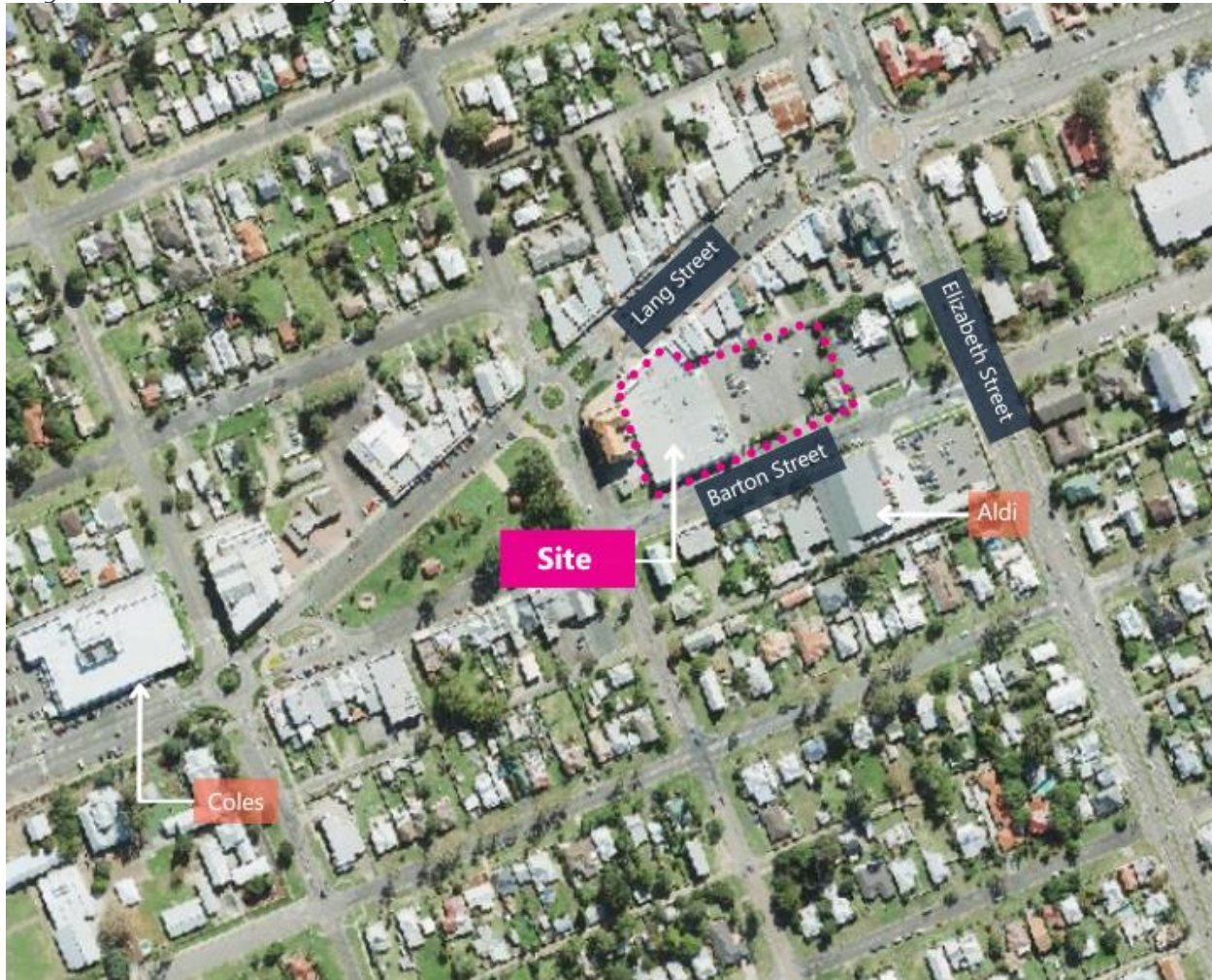
This consultation outcomes report summarises the consultation undertaken between 30 June 2023 and 5 August 2023.

The consultation process was designed to give the community and stakeholders an opportunity to express their concerns, for the Proponent to consider design changes based on feedback prior to submission of the updated DA.

Project location

The site is known as Kingsway Plaza Shopping Centre, at 174-174 Lang Street, Kurri Kurri, 2327, located within the Cessnock Local Government Area (LGA). The site is legally described as Lot 136 on DP 869710 and sits on approximately 3,200 square metres of existing retail floor space and car parking for 92 vehicles. The site fronts the Lang Street, while the carpark entrance is off Barton Street. The eastern Lang Street boundary adjoins retail properties, and the western boundary adjoins a hotel. There is also an existing laneway which hugs the carpark, exiting onto Victoria Street. The site is surrounded by single and two (2) storey buildings of residential and business and retail uses.

Image 1: Site map 174-178 Lang Street, Kurri Kurri



2. Consultation process

Approach

The consultation plan was designed to respond to Council's request for further consultation, specifically:

- provide clear and concise information about the project and its impacts
- implement activities that encourage and facilitate participation
- how matters raised (particularly the direct impacts) are addressed under any amendments made to the proposal.

Consultation objectives

The aim of the consultation is described in **Table 1** and summarises the activities undertaken to provide stakeholders and the community genuine opportunities to participate in the project.

Table 1: Consultation objectives

Objective	How this was done
Identify key stakeholders for the proposal and their respective requirements	Stakeholder mapping was conducted to understand and inform when and how consultation would occur. A range of tools were used to encourage participation, including face-to-face, online and printed. A 300 m radius was used for letterboxing to inform stakeholders of engagement activities.
Make the community and stakeholders aware of the project	Preparation of project information including developing a project specific website, images and maps, fact sheets, emails, letterbox drop, door knocks, emails, phone calls. Stakeholder meetings were also held with Councillors.
Distribute information to residents, stakeholders, neighbours, local businesses, impacted stakeholders, and broader community	Distribution of project information via one-on-one meetings, letterbox drops and mail outs, online consultation platform web pages, phone calls, online and in person community consultation sessions, emails.
Identify and define potential issues and opportunities arising from the proposed development	Use feedback provided through the various feedback channels to identify and define potential issues and update project information, fact sheets, and FAQs. Feedback was used to inform changes to the design.
Provide an opportunity for the community and stakeholders to provide feedback	Gathered feedback via project-based contact channels including email address and mailbox, online surveys, community drop-in session, phone calls, meetings.



Stakeholder identification and engagement

Stakeholders were included in the consultation plan based on their geographic proximity to the project as well as potential impacts identified as part of the development application (including potential noise, traffic and transport, visual impacts of the proposed development). Stakeholders consulted during the post-lodgement engagement included:

- approximately 350 landowners, residents, businesses who live up to 300 m of the proposed development
- key stakeholders such as government agencies and authorities, Council, and the Hunter Central Coast Regional Planning panel
- anyone who had previously registered for communication updates about the project.

Summary of consultation activities

The consultation was live between 3 July 2023 to 5 August 2023 to ensure people had sufficient time to provide feedback, raise issues and concerns. A range of channels were established to provide stakeholders with an opportunity to give feedback or get in contact at any point during the consultation. It is anticipated there will be an additional formal exhibition period once Higgins Planning has revised the development proposal and responded to the request for information.

Table 2: Contact channels used

Mara communication channel	Details
Project information email	projects@maraconsulting.com.au
Project website	https://www.maraconsulting.com.au/kingswayplaza
Facebook	https://fb.watch/mBrQIeT4LB/
LinkedIn	https://www.linkedin.com/feed/update/urn:li:activity:7081526067722653696

The consultation process has provided opportunities for stakeholders and members of the community to learn about the project and for the proponent to capture and respond to the matters being raised. Table 3 provides a description of the consultation tools and materials that were developed.

Table 3: Consultation tools

Tools	Description	Stakeholder groups
Community information session	One community drop-in session was held at Kingsway Plaza on Thursday 13 July 2023 between 3:00pm till 6:00pm. This was an opportunity for engagement between interested and impacted stakeholders, and the project team. There was a discussion on what issues had been heard to date and it was an opportunity to identify concerns and opportunities.	Residents, businesses, community and near neighbours.
Website	Provided project information to community and stakeholders and gathered feedback on issues, and opportunities regarding the project. The site went live on 3 July 2023 and still hosts project information.	All stakeholders
Contact mechanisms	A variety of contact mechanisms were established including project email, dedicated post box and website. Contact details were included on all project-related communication materials.	All stakeholders
Communications (emails, factsheets, frequently asked questions)	Distribution of project information to community and stakeholders via letterbox drops, email distribution, web pages.	All stakeholders
Briefings	Briefings/forums with key agency and government stakeholders to seek feedback on a specific issue or range of issues. Both online and in-person.	Cessnock City Council, Joint Regional Planning Panel (JRPP), Local MPs UDIA and RDA.
Meetings	Meetings with key agency and council stakeholders to seek feedback on the proposal. Meeting dates are included in Table 5: Summary of post-lodgement consultation to meet Council's RFI.	Cessnock City Council, JRPP
Survey	Online survey to give the community the opportunity to raise issues, concerns or opportunities.	All interested stakeholders
Feedback form	A formal way for attendees to the drop-in session to provide feedback on the process as well as raise concerns or opportunities.	Attendees to the drop-in session.

Table 4 summaries the approximate numbers of stakeholders engaged through the post-lodgement activities.

Table 4: Contact statistics between 3 July and 23 August 2023

Activity	Number of contacts
Website visitors	560 (as of 23/08/2023)
Emails (in and out)	50
Community in-person drop-in sessions at Kingsway Plaza	1
Attendance to community drop-in sessions	44
Letterbox drop (properties) within 300 m of project site	~350
Phone calls	26
Survey	65 responses

How and when consultation occurred

Consultation occurred in two distinct phases; pre-lodgement, post lodgement reintroduction of the project with stakeholders to understand concerns and key issues. As a result of phase 3 engagement, the masterplan was further revised post consultation prior to resubmitting to Council for consideration. Refer to Table 5 for further detail about the consultation.

1. Pre-lodgement – xxxxxx

Higgins Planning conducted consultation with relevant stakeholders and community representatives, existing residents of , and neighbours to identify issues to inform the social impact assessment and the DA. Other activities included:

- Phone calls with neighbouring properties
- Meetings with businesses, residents and written communications as a follow-up
- Pre DA meetings with Cessnock City Council
- Meetings with Hunter & Central Coast Regional Planning Panel.

For details, refer to Higgin Planning's Social Impact Assessment and SEE.

2. Post lodgement – 3 July 2023 to 5 August 2023

The focus of this phase was to provide information to businesses, residents, directly impacted stakeholders and the broader community about the Kingsway Plaza and encourage feedback on the project, identify issues and opportunities.

Table 5: Summary of post-lodgement consultation to meet Council's RFI

DATE	CONSULTATION EVENT	STAKEHOLDERS	DESCRIPTION
19/10/22 03/03/23 14/03/23 15/03/23	Emails	Tenants	Meeting with tenants about the proposed development and managing construction impacts.

DATE	CONSULTATION EVENT	STAKEHOLDERS	DESCRIPTION
04/04/23	Meeting	Hunter & Central Coast Regional Planning Panel	Overview of the proposed development application including key issues for consideration.
05/05/23	Meeting	Cessnock City Council	Meeting to discuss Development Proposed Redevelopment of Existing Shopping Centre. Issues raised: <ul style="list-style-type: none"> • Voluntary Planning Agreement • Phasing of development • Construction and traffic management plan • Laneway circulation/traffic • Barton Street • Plaza • Public art • Adjoining heritage building • Demolition/earthworks • CPTED Assessment • SIA/Community consultation
06/06/23	Online meeting	Hunter & Central Coast Regional Planning Panel	Update on proposed development application including key issues for consideration. Issues raised: <ul style="list-style-type: none"> • Voluntary Planning Agreement • Traffic circulation and laneway • Pedestrian linkages • Murals to be replaced • Land dedication • SIA does not meet Council's policy. CPTED report to be updated. Consultation is required.
09/06/23	Email	Towns with Heart	Feedback sought from President of Towns With Heart, responsible for the murals around Kurri Kurri, to understand what a suitable replacement would be for the murals to be demolished as part of the development application.
28/06/23 to 03/07/23	Phone calls	Local MPs and Councillors	Rang to introduce the project and advise consultation was open. A follow up email provided with project information.
28/06/23 to 03/07/23	Phone calls	Local businesses	Rang to introduce the project and advise consultation was open. A follow up email provided with project information.
03/07/23	Emails	Local businesses	Emailed project information with link to survey and website, as well as invitation to drop-in session.
03/07/23	Survey	All stakeholders	Online survey to gather feedback on the proposal. Sixty-five people completed the survey, with an overwhelming level of support
06/07/23	Door knock	Local businesses	Door knocked approximately 100 businesses around the development providing project information and inviting to drop-in session

DATE	CONSULTATION EVENT	STAKEHOLDERS	DESCRIPTION
06/07/23	Letterboxed	Local businesses and residents	Mail delivery to approximately 350 residents and businesses within 300 m of the proposed development advertising information sessions and how to make a submission or provide feedback.
07/07/23	Meeting	Cessnock City Councillors	Meeting to discuss the proposal and answer questions.
08/08/23	Email	Councillor Sander	Follow up on telephone conversation in relation to future leasing opportunities at Kingsway Plaza.
13/08/23	Drop-in session	All interested stakeholders	Forty-four people attended the drop-in session at Kingsway Plaza. Architectural drawings, renders and floorplans were available. Attendees asked questions and talked with the project team. Feedback forms were available.

3. Feedback

During the consultation process, stakeholders identified a range of concerns regarding the project. Although feedback has differed based on individual stakeholder interests, several consistent issues emerged.

Summary of feedback and issues raised

Table 6: Summary of issues and responses provides a summary of these issues and opportunities as raised by the stakeholder, together with a response and or/reference to where these issues are addressed in this development application. The themes and issues are not presented in priority order. It is, however, noted that the most raised issues by stakeholders related to proximity, local character, and traffic.

Table 6: Summary of issues and responses

ISSUE	HOW ADDRESSED
Council provided a DA request for information (RFI) which covered issues for consideration by the proponent.	A detailed response to the RFI will be provided to Council outlining how each item is being addressed by Higgins Planning. Please refer to the RFI document and technical responses from the broader project team.
What will happen to the existing tenants?	Existing leases have come to an end and are currently running are month to month. The existing tenants are welcome to make contact with the current leasing agents to provide their expression of interest to be future tenants.
What is the construction timeframe?	The process involves gaining development consent, preparing tender documents, tendering the project and appointing a principal contractor (builder), principal contractor preparing construction certificate documents an various supporting application under Roads Act and Local Government Act, demolition commencing, laneway being extended, laneway extension land being handed over to Council for a public road, existing laneway being closed, construction commencing on new building and associated works, construction process being concluded and occupation certificates being issued, Woolworths fit out being completed and store opening. (Please adjust as necessary and advise how many months for each step in the process upon receipt of Development Consent – thanks, Marian)
Will there be other shops apart from Woolworths?	Yes, the original and amended drawings include Tenancy 1 (Spec T1) – 350m2 on upper level and unchanged in amended DA (no specific tenant known at this stage, Tenancy 2 (Spec T2) – 100m2 (no specific tenant known at this stage) on lower level and unchanged in amended DA, and Tenancy 3 (Spec T3) - existing pharmacy tenancy space reduced due to extended laneway to 200m2 on upper level fronting Lang Street.

Will there be toilets?	An accessible water closet has always been labelled on the lower ground level "Acc WC" and is shown on the Amended Drawings lower ground level DA10.
How many carpark spaces will there be?	In total there will be 177 car parking spaces: <ul style="list-style-type: none"> • 146 lower level • 31 upper level.
When will it be built?	Construction is expected to begin with 6 months of approval and subject to market conditions.
How will cars get into the development?	As detailed in the Amended Drawings Access into the site is proposed from the extended laneway and Barton Street for vehicles
Questions were raised about traffic management and concerns about traffic impacts through construction.	<p>If the Development Application is approved, a traffic management and safety plan will be developed.</p> <p>Traffic engineers have, completed a traffic study and report as part of the Development Application and updated as part of the RFI process. The traffic management advice is reflected in the revised masterplan.</p>
Will the murals be replaced?	Yes. The project team has been working with Towns with Heart to identify an appropriate location for a new mural. The developer will consult with Towns with Heart to design a new mural.
What security cameras and lighting will be in place?	<p>Woolworths will have CCTV coverage for their entry and internal areas. There will be provision for coverage of the ground floor lobby and car park to be monitored back to the Woolworths base system.</p> <p>The basement and public areas will be lit in accordance with Australian Standards.</p>
Council raised concerns that there was insufficient consultation prior	In June 2023, Mara Consulting was engaged to support the request for further consultation. This report outlines the activities to consult and gather feedback to inform the revised development proposal.

Survey

A survey was open to the community between 3 July and 5 August 2023. A total of 65 responses were received during this time. Survey responses identified the following:

- the majority of respondents were aware of the project (97 per cent)
- on balance respondents were supportive of the proposal, with 85 per cent very supportive of the redevelopment
- most respondents agreed the project was important to the local economy (95 per cent)
- respondents indicated the develop would have a positive impact on:
 - how the site is currently used (46 per cent)
 - the way people travel around the area (55 per cent)
 - the way that people might use new connections through the site, linking them to the surrounding community (77 per cent)
 - access to services and jobs (88 per cent)
 - sense of community (77 per cent)
 - how Kingsway Plaza redevelopment project will integrate with the surrounding community (79 per cent)
 - the way that people might use new connections through the site, linking them to the surrounding community (77 per cent)
 - health and wellbeing (73 per cent)
 - what people list most about living in the area (79 per cent)
 - built form – the look and feeling of buildings (78 per cent)
 - visual impact, views and sightlines (70 per cent)
 - road transport and traffic in the area (39 per cent)
 - economic investment in the area (90 per cent)
 - construction jobs and business opportunities (88 per cent)
 - ongoing jobs and business opportunities through operation (95 per cent)
- respondents indicated a neutral position in relation to:
 - Aboriginal cultural heritage (70 per cent)
 - European heritage (64 per cent)
 - noise (53 per cent)
 - dust through construction (48 per cent)
 - natural environment (48 per cent)
- respondents indicated the develop would have a native impact on:
 - Noise, dust through construction, visual impact, traffic, habitat, and environment.
- participants indicated providing feedback (91 per cent) was important and they had the ability to participate (73 per cent)
- on balance **96 per cent** of respondents believe the Kingsway Paza redevelopment will be positive for Kurri Kurri and the region.

What respondents said

- “Coles and Aldi are the only major retail stores in Kurri. Coles is heavily congested all the time now with all the new sub divisions in place and planned. Woolies will create more opportunities for locals choosing where they shop”
- “It will bring jobs, and more important provide people with a reason to visit our Main Street. It will bring passing tourists to our Main Street and that can only be a positive for the surrounding retailers”
- “Will create jobs and boost the local economy, as well as provide competition to the other two supermarkets in the area.”
- “It's a major building that is not being utilised in the centre of town. Something needs to be put there to create that hub that used to exist.”
- “Local jobs will be amazing and it will be good for smaller business as I feel it will bring more people into Kurri. eg Instead of Heddon Greta people travelling into Maitland or Abermain people travelling to Cessnock to do there shopping at Woolworths”
- “It will provide many much needed employment opportunities and much needed competition for shopping”.

Summary of changes based on consultation

The follow table outlines the evolution of the masterplan based on the feedback gathered from stakeholders.

Table 7: Evolution of the masterplan for

ORIGINAL PROPOSAL (DA SUBMITTAL)
<p>The original design included:</p> <ul style="list-style-type: none"> • demolition of existing structures and site works; • part road closure, relocation, and extension of existing laneway road with associated subdivision; • alterations and additions to the Lang Street existing pharmacy building, • construction of a new commercial buildings with two specialty shops and new Woolworths supermarket store usage and fit out, including: <ul style="list-style-type: none"> ○ mezzanine offices, plant room and plant deck ○ car parking ○ “Direct to Boot” bays and storage ○ signage , publicly accessible plaza, and public art space ○ loading facilities ○ landscaping.

LANEWAY CHANGES AND IMPROVEMENTS

- Laneway widened to 5.5m width to promote a 2-way movement in a low-speed environment (width is 5.5m for full length except in locations where there are existing encroachments as shown in the attached detailed survey). The locations of the encroachments in the existing laneway include:
 - rear of café buildings on Lot 24 DP 544418 and Lot A DP 373375
 - guard rail into roadway on Lots 8 and 9 Section 20 DP 758590
 - rear of building at northern end of laneway on Lot 5 Section 20 DP 758590
- Extended laneway heading south with widened corner turn movement for suitable swept paths.
- Laneway design change with wider turn at Plaza junction.
- New pedestrian crossing design (levels and ramping grades all as per previous DA).
- Site Staging Plan with Stage 1 demolition works to existing Kingsway Plaza building with retention / make good of existing retail building T3 fronting Lang Street, Stage 2a construction of extended laneway and handover from landowner to Council while maintaining existing laneway, Stage 2b closure of existing laneway and handover from Council to landowner, Stage 3 construction of new retail building over lower level car parking area.
- Area of proposed closed section of laneway reduced from as originally submitted 459m² to 454m², area of extended laneway increased from as originally submitted 578m² to 647m².

Council advised via email dated 2 August 2023 following a number of meetings with the applicant's traffic engineer:

"Council is satisfied with the existing laneway remaining two-way, notwithstanding the non-compliance with the width. This is on the basis that the laneway currently operates in a two-way manner, and is a very low speed environment."

DESIGN CHANGES TO BUILDINGS

- Adjustments to lower-level carpark design to suit amended laneway boundary set out.
- Adjustments to the car parking layout including:
 - Lower ground level reduced from originally submitted 147 spaces (inclusive of 113 spaces, 3 disabled parking spaces, 6 direct to boot bays, 25 on-street parking spaces and 7 trolley bays to 146 (inclusive of 115 spaces, 2 disabled parking spaces, 6 direct to boot bays, 25 on-street parking spaces and 7 trolley bays) in the amended architectural drawings.
 - At-grade upper level maintained at from originally submitted 33 spaces (inclusive of 32 spaces plus 1 disabled space) to 31 spaces (inclusive of 29 spaces plus 2 disabled spaces) in amended drawings.
- As a result of the widening of the laneway, the trading floor of the proposed supermarket was reduced along the western boundary by 300mm. The Gross Floor Area (GFA) of the supermarket reduced from original submitted 3,399m² to 3,375m².
- The overall GFA reduced from original submitted 4,621m² to 4,607m².

- The northern setback for the speciality retail T1 to the front of the supermarket has an increased setback from the laneway from 1m to 1.3m with the inclusion of shop front glazing integrated to better address the laneway.
- Egress stair from lower-level car parking level to plaza location adjusted.

PUBLIC DOMAIN WORKS - LANG STREET PLAZA

- Plaza area is now defined on 4 sides with planter boxes at the corner pushed back to define the southern edge of the plaza
- Council's preferred paving (bluestone) has been integrated into the scheme, with the new planter box assisting to define the edge of the new and old paving until the rest of the area is reconstructed
- The pavement through the corner will be made consistent with the roadway given the anticipated traffic demands. To assist slowing traffic and defining the pedestrian spaces, bollards edge the roadway and a wider wombat style crossing integrated along the main pedestrian desire line
- Large deep soil tree planting integrated into the plaza
- The edge of the brick planter supporting the tree forms a long seating edge
- Street furniture integrated into the plaza (lighting and waste receptacles).

BARTON STREET

- The proposed public domain plan from council has been integrated into the street with a line of street trees interspersed among the 90-degree parking to provide street tree canopy
- Wider paved areas are shown at the main entry to the retail carpark and lift, along with the speciality retail
- The pergola structure has been extended over the basement carpark ramp areas to assist in defining the edge of the plaza space and screen downward views to the carpark from the hotel.

BUILDING FAÇADE

Several key changes have been made to the building design

- Main roof pitch has been reorientated to lower the façade height along Barton Street by 1-2m. This reduced the bulk as presenting to the street and to the new laneway to the north
- Oxide panels have been extended around and above the brick datum to break up the length of the Barton elevation
- The roof planes of the mezzanine area have been expressed in the external façade to break up the silhouette of the main façade
- The roof line to the lift has been pitched to break up the silhouette of the building
- Planter boxes have been extended along the length of the upper level at-grade carpark edge to Barton street to create a longer green presentation to the street.

4. Outcomes report summary

This outcomes report is a summary of the activities completed and feedback received through the consultation program to support the post-lodgement response to the request for information and to build on consultation completed prior to lodging the development application for the Kingsway Plaza Redevelopment.

Council requested as a part of the RFI in May 2021, consultation should be undertaken given the *“the applicant did not undertake consultation prior to lodgement of the development application”*.

Subsequently, Mara was commissioned to conduct consultation to meet this request, which occurred between July 2023 and August 2023. The proponent also conducted consultation to support the application, particularly with key stakeholders (tenants, adjacent property owners).

In the RFI, Council requested that targeted consultation with all affected property owners as well as adequate community consultation. This report seeks to provide a formal response to the RFI highlighting issues that have been raised in the submissions, including how the matters raised have been addressed in the amended proposal.

The intention of the engagement was to address community concerns about the proposal and specifically the amount of feedback received during the consultation. The process was not to seek consensus on the proposal, but rather provide sufficient opportunity for the community to participate in the project. A range of communication methods were used to promote participation and three rounds of consultation were conducted.

Stakeholders were included in the consultation based on their geographic proximity to the project as well as potential impacts identified as part of the development application (including potential noise, traffic and transport, visual impacts of the proposed development). Stakeholders consulted included approximately 350 businesses and residents, businesses who live within 300 m of the proposed development.

As of 25 August 2023, more than 560 people had visited the project website seeking information about the project. Additionally, 44 people attended the drop-in session at Kingsway Plaza. Promotion of the engagement events were via phone calls, a letterbox drop, door knocking, emails providing a project overview and information about how to participate.

Additionally, feedback received from participants indicated that overall, 71% of participants were very satisfied with the consultation process, while the remainder were somewhat satisfied. Over 95 per cent of participants indicated they had their questions answered, indicating that there were very high levels of satisfaction with the engagement.

It is understood there will be a further exhibition period proposal giving the community another opportunity to formally make a submission or provide feedback. Council is responsible for notifying surrounding neighbours and will advertise the exhibition period. Community members will have the ability to make a formal submission on the proposal to Council and will be considered as part of the determination by the Panel.



The project team will continue to provide information to the community and stakeholders about ongoing activities including project updates at key stages.

This outcomes report concludes that while a small number of potential impacts have been raised by the community, with mitigation and management, ongoing residual impacts are manageable. Feedback gathered through the consultation was overwhelmingly in support of the redevelopment of the Kingsway Plaza.



Appendices

Appendix A – Factsheet

Appendix B – Sample community information session

Appendix C – Feedback form

Appendix D – Sample of website

Appendix A - Factsheet



ABOUT

Have your say

The Kingsway Plaza in Lang Street Kurri Kurri is proposed to be redeveloped. The community is invited to provide feedback on the proposed development.

The proposal is to create a new shopping centre with three specialty shops and a Woolworths supermarket. The centre will include undercover car parking for approximately 154 vehicles, over two levels.

The image below shows an extended and relocated laneway that will connect Barton Street and Victoria Street.

The laneway will also connect to Lang Street via a pedestrian plaza.

The proposed develop will also include a lift to Barton Street, travelators, signage, a new mural and landscaping around the site.



Image: Artist impression of proposed development.



GET IN TOUCH

W: maraconsulting.com.au
E: projects@maraconsulting.com.au

TAKE THE SURVEY



FREQUENTLY ASKED QUESTIONS

What is the proposal?

The project is a retail development includes a Woolworths supermarket, three retail stores, parking, road works, signage, landscaping and a new mural.

Will there be any local job opportunities?

The proposed development is likely to create approximately 16 full-time jobs through construction. Additionally, there will be approximately 150 retail jobs created once operational.

What are the hours of operation?

The proposed hours of trade include:

- **Woolworths supermarket**
Monday to Sunday
6am to 12am (midnight)
- **Specialty shops (general)**
Mondays to Sunday
9am to 6pm
- **Loading dock**
Monday to Sunday
5am to 12am.

What impacts will there be?

During construction there will be temporary disruptions to traffic and pedestrian access. There will be some noise and vibration associated with the work and mitigation measures will be used to reduce impacts. Updates will be provided to the community through construction phase.

Have your say

Will there be any benefits for the community from the proposal?

The additional 150 retail jobs are estimated to be an additional \$6.8 million in salaries and wages for the local economy as a direct result of the proposal. Additionally, the new buildings will improve the amenity of the area, and the pedestrian access, connecting Lang Street with the development and laneway. The proposal includes three retail spaces for businesses in addition to the supermarket.

How can I provide feedback on the proposal?

We are currently seeking community feedback on the proposed development. To get involved:

- Visit maraconsulting.com.au
- Take the survey using the QR code below
- Email projects@maraconsulting.com.au
- Attend the drop-in session to ask questions and give feedback.
When: Thursday 13 July 2023
Time: 3:00pm till 6:00pm
Where: Kingsway Plaza

This feedback will be used to inform a Development Application (DA) for submission to Cessnock Council.

JOIN THE CONVERSATION

Feedback gathered through the consultation will be included in a report as part of the Development Application (DA) for the proposed project and submitted to Cessnock City Council for assessment. Mara Consulting is conducting the consultation on behalf of the developer.

Visit the drop-in session to find out more:

When: Thursday 13 July 2023
Time: 3:00pm till 6:00pm
Where: Kingsway Plaza



GET IN TOUCH

W: maraconsulting.com.au
E: projects@maraconsulting.com.au

TAKE THE SURVEY

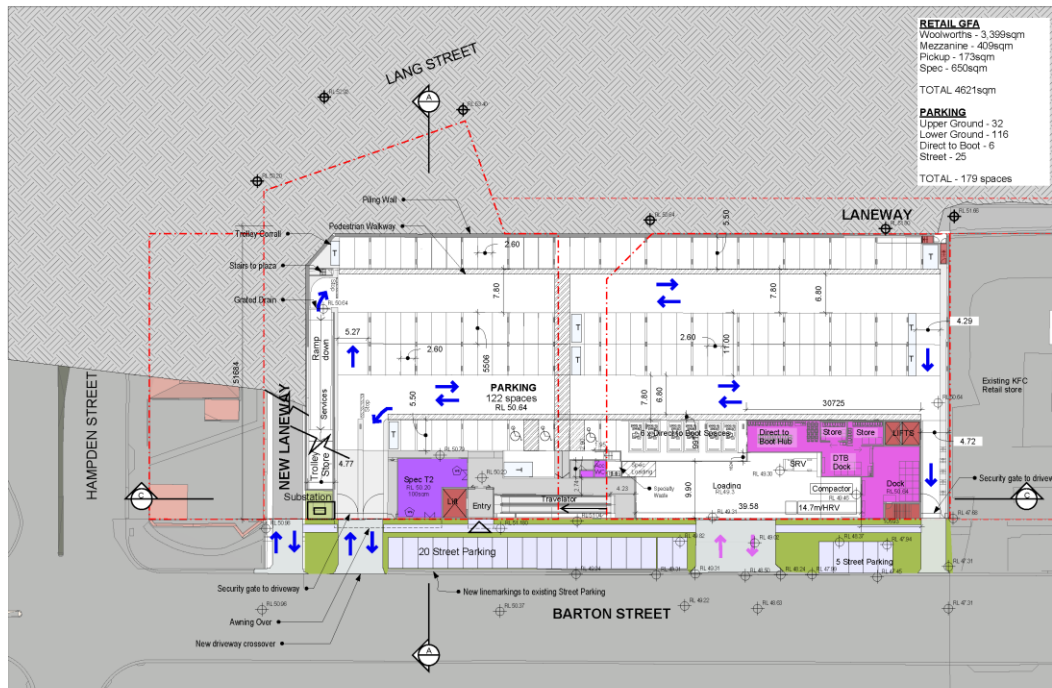


Appendix B - Sample Drop-in session information

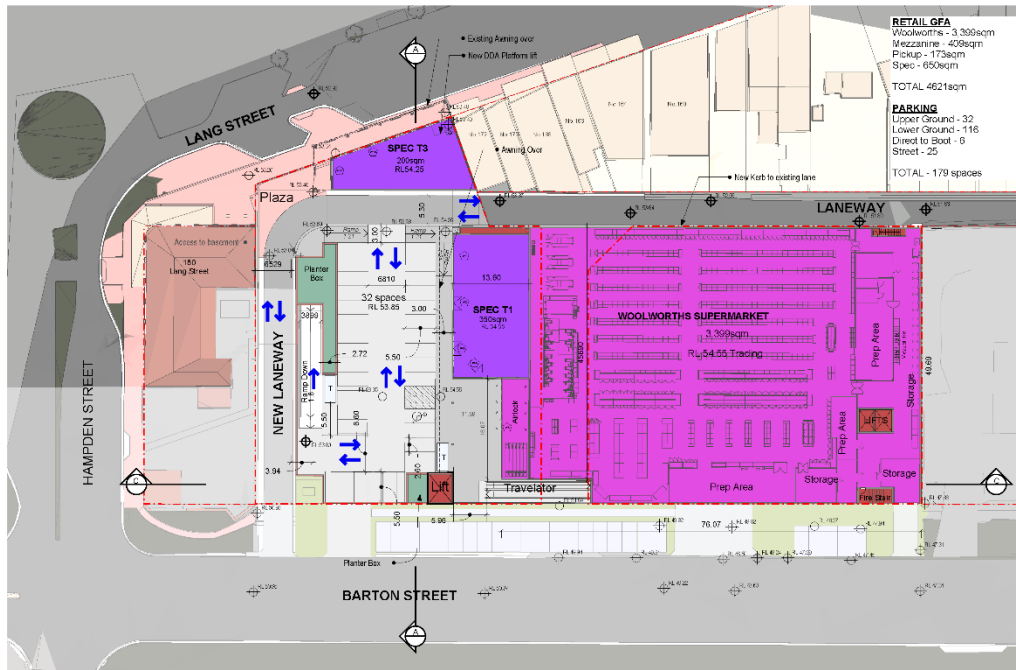




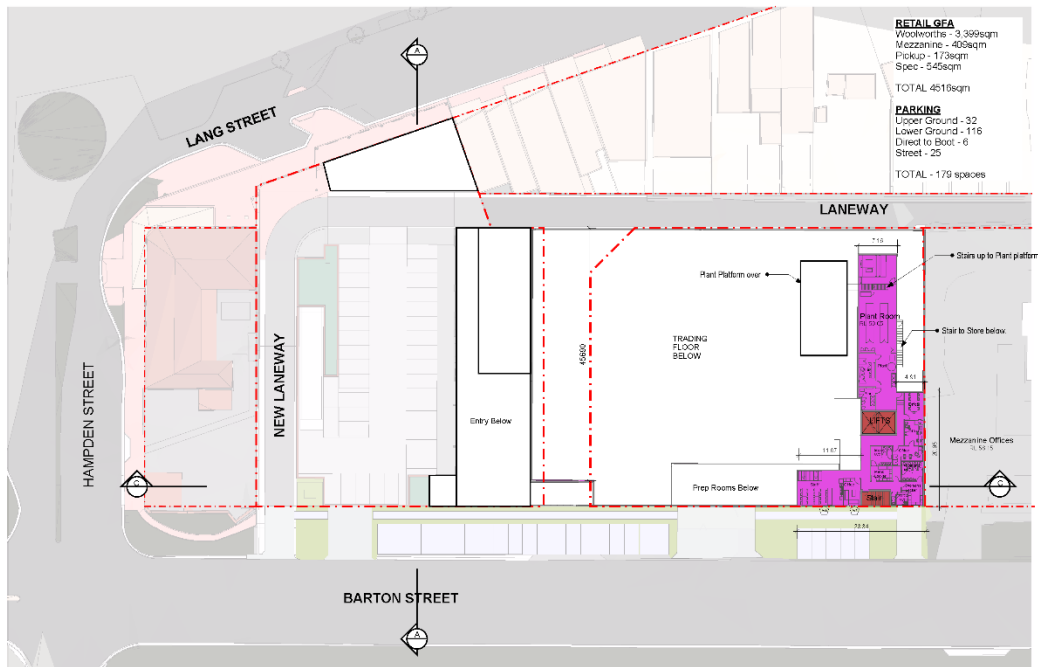
VOTRANT NO124 PTY LTD PROJECT NO. 2201		This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Finished ground levels on the plan are subject to site conditions. Do not scale from drawings. Figured dimensions are to be given preference over scale.	PROJECT TITLE 174-176 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title Drawing Scale Issue Date Revision	SITE PLAN 1:100@A3 31/01/2023 DA02
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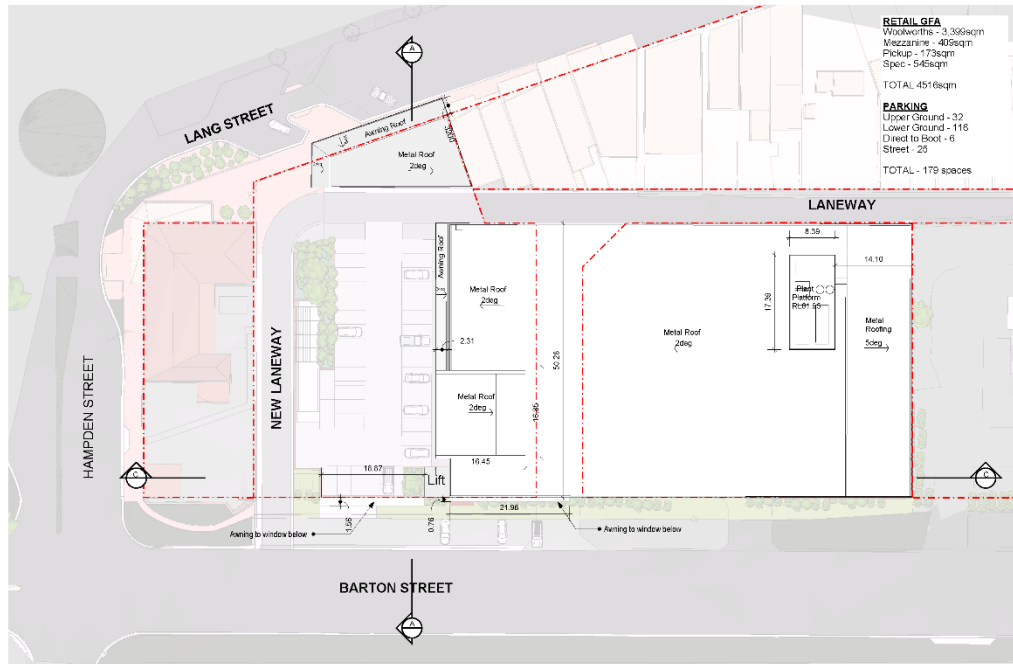
VOTRANT NO124 PTY LTD PROJECT NO. 2201		This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Finished ground levels on the plan are subject to site conditions. Do not scale from drawings. Figured dimensions are to be given preference over scale.	PROJECT TITLE 174-176 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title Drawing Scale Issue Date Revision	LOWER GROUND FLOOR 1:500@A3 31/01/2023 DA10
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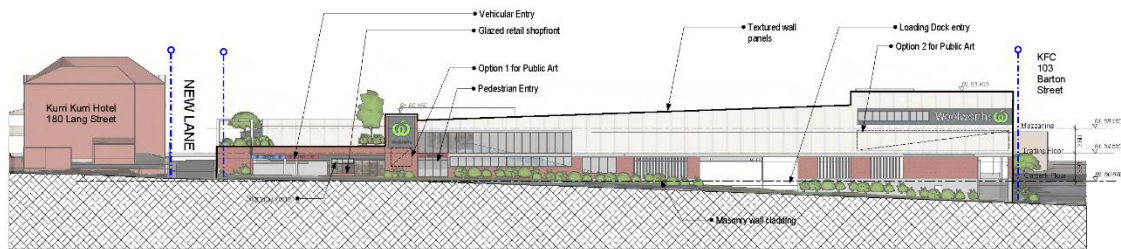
VOTRANT NO154FTY LTD PROJECT NO. 2201		This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Further ground levels on this plan are subject to site conditions. Do not scale from drawings. Figure dimensions are to be given preference over scale.	PROJECT TITLE 174-178 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title UPPER GROUND FLOOR Drawing Scale 1:500@A3 Issue Date 31/01/2023 Revision A	Drawing DA11
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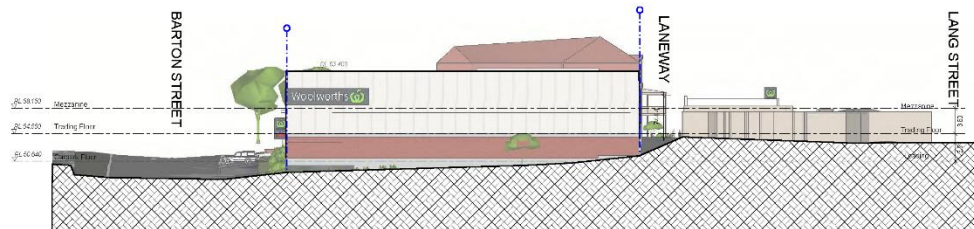
VOTRANT NO154FTY LTD PROJECT NO. 2201		This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Further ground levels on this plan are subject to site conditions. Do not scale from drawings. Figure dimensions are to be given preference over scale.	PROJECT TITLE 174-178 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title MEZZANINE FLOOR Drawing Scale 1:500@A3 Issue Date 31/01/2023 Revision A	Drawing DA12
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VOTRANT 10134 PTY LTD PROJECT NO. 2201		This drawing is protected by copyright. It may only be used for the purposes for which it was issued. Fenced ground levels on the plan are subject to site conditions. Do not scale from drawings. Figure dimensions are to be given preference over scale.	PROJECT TITLE: 174-178 LANG STREET, KURRI KURRI ISSUED FOR: DEVELOPMENT APPLICATION Drawing Title: Drawing Scale: 1:500 Issue Date: 31/01/2023 Revision: A	ROOF LEVEL: 1500@A3 Drawing: DA13
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SOUTH ELEVATION (BARTON STREET)

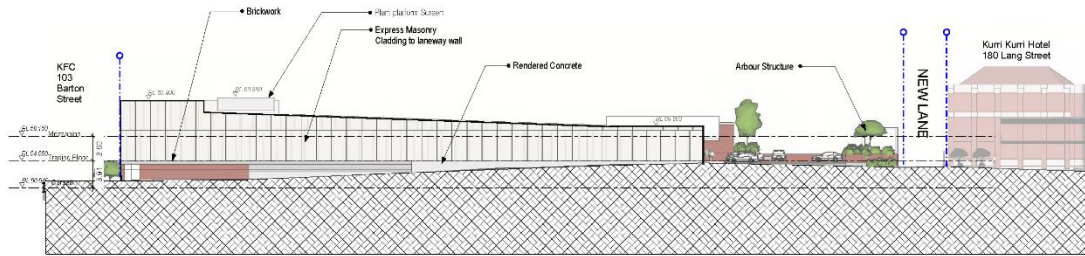


EAST ELEVATION

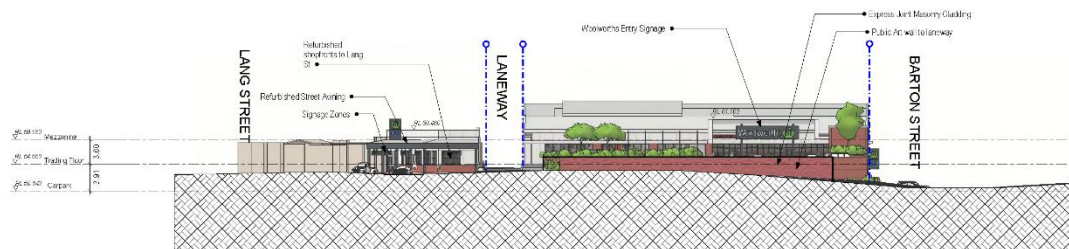
MATERIALS (Legend)

DRK1 - Brickwork	PT1 - White Painted Finish
DRK1 - Masonry Wall Panels	GL1 - Glass Panels
DRK2 - Concrete Textured	PD1 - Powdercoated Vertical Fin Series
DRK3 - Concrete Blockwork	PD2 - Powdercoated Screens

VOTRANT 10134 PTY LTD PROJECT NO. 2201	This drawing is protected by copyright. It may only be used for the purposes for which it was issued. Fenced ground levels on the plan are subject to site conditions. Do not scale from drawings. Figure dimensions are to be given preference over scale.	PROJECT TITLE: 174-178 LANG STREET, KURRI KURRI ISSUED FOR: DEVELOPMENT APPLICATION Drawing Title: ELEVATIONS - SOUTH & EAST Drawing Scale: 1:400 Issue Date: 31/01/2023 Revision: A	Drawing: DA20
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NORTH ELEVATION (LANEWAY)

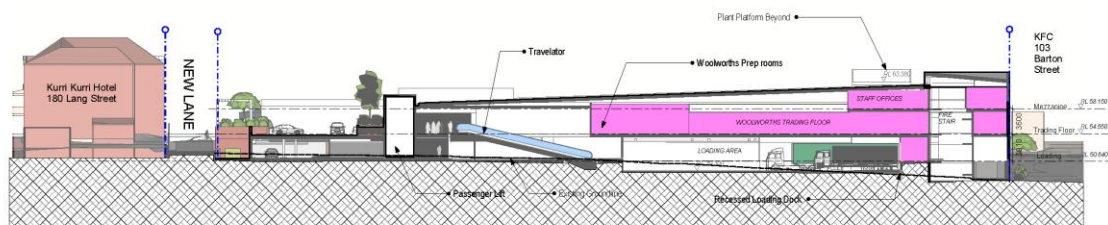


WEST ELEVATION (NEW LANEWAY)

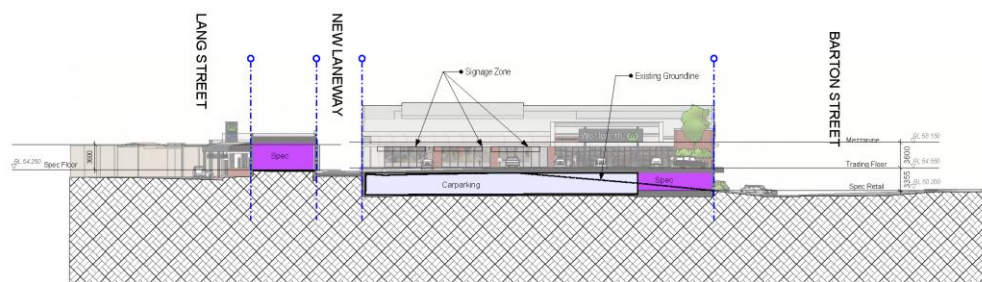
MATERIALS LEGEND

BRW1 - Brickwork	PT1 - White Painted Finish
CON1 - Masonry Wall Panels	GL1 - Glass Panels
CON2 - Concrete Tied/Joint	PD1 - Powdercoated Window Frames
CON3 - Concrete Blockwork	PD2 - Powdercoated Screens

CURRAMULLEN PTY LTD PROJECT NO 2201	This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Printed ground levels on the plan are subject to site conditions. Do not scale from drawings. Figure dimensions are to be given preference over scale.	PROJECT TITLE 174-178 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title ELEVATIONS-NORTH & WEST Drawing Scale 1:400@A3 Issue Date 31/01/2023 Revision DA21
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SECTION C-C



SECTION A-A

CURRAMULLEN PTY LTD PROJECT NO 2201	This drawing is protected by copyright. It may only be used for the purpose for which it was issued. Printed ground levels on the plan are subject to site conditions. Do not scale from drawings. Figure dimensions are to be given preference over scale.	PROJECT TITLE 174-178 LANG STREET, KURRI KURRI ISSUED FOR DEVELOPMENT APPLICATION	Drawing Title SECTIONS-AA & CC Drawing Scale 1:400@A3 Issue Date 31/01/2023 Revision DA22
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Appendix C – Feedback form

Kingsway Plaza Redevelopment proposal

Feedback form

13 July 2023

Thank you for attending our community event. We welcome your feedback and input into the Kingsway Plaza redevelopment proposal. Feedback gathered through the consultation will be included in a report as part of the Development Application (DA) for the proposed project and submitted to Cessnock City Council for assessment.

General feedback

1. Overall, how satisfied are you with the level of information about the Kingsway Plaza redevelopment proposal?

☐ Dissatisfied ☐ Somewhat dissatisfied ☐ Neither ☐ Somewhat satisfied ☐ Very satisfied ☐ Not sure or NA

Please read the following statements and indicate your level of agreement.

2. The information about Kingsway Plaza redevelopment proposal presented today, have answered my questions.

☐ Strongly disagree ☐ Disagree ☐ Neither ☐ Agree ☐ Strongly agree ☐ Not sure or NA

3. I had the opportunity to express concerns and ask questions of the project team.

☐ Strongly disagree ☐ Disagree ☐ Neither ☐ Agree ☐ Strongly agree ☐ Not sure or NA

4. A new supermarket and updated retail offerings are needed in the area.

☐ Strongly disagree ☐ Disagree ☐ Neither ☐ Agree ☐ Strongly agree ☐ Not sure or NA

5. On balance, the proposal will benefit the community.

☐ Strongly disagree ☐ Disagree ☐ Neither ☐ Agree ☐ Strongly agree ☐ Not sure or NA

6. Did you find the information session useful?

☐ Yes ☐ No Why?

7. Other comments

PTO >>>

Get in touch

E projects@maraconsulting.com.au
W <https://www.maraconsulting.com.au/kingswayplaza>





[illegible]

Is a follow up from today's event required? ☐ Yes ☐ No Please specify.....



Appendix D - Sample of website (home page)


[Home](#)
[About](#)
[Services](#)
[Projects](#)
[News](#)
[Contact](#)
[Contact us](#)



Proposed development

Kingsway Plaza Redevelopment
(74-178 Long Street, Kurri Kurri)

Have your say

Kingsway Plaza Redevelopment

The Kingsway Plaza in Long Street Kurri Kurri is proposed to be redeveloped. This page outlines the project and explains how to provide feedback on the proposed development.

About the project

The Kingsway Plaza Redevelopment proposal is to create a new shopping centre with three specialty shops and a Woolworths supermarket. The centre will include undercover car parking for approximately 154 vehicles, over two levels.

The existing laneway will be extended and relocated laneway connecting Barton Street and Victoria Street. The laneway will also connect to Long Street via a pedestrian plaza. The proposed develop will also include a lift to Barton Street, travelers, signage, public art and landscaping around the site.





Image: Artist impression of the Kingsway Plaza showing redesigned laneway.

Artists Impressions



Frequently Asked Questions

<p>Why is the Kingsway Plaza being redeveloped?</p>	<p>Unfortunately, the current building is outdated, with inadequate amenities and does not meet the needs of tenants or the surrounding community. When IGA vacated the site in 2021, there was an opportunity to revitalise the site, improve the amenity of the area and stimulate the economy. It is also an opportunity to design a building that meets current environmental sustainability measures in both the building and the operations of the tenancies.</p>
<p>Will there be any local job opportunities?</p>	<p>The proposed development is likely to create approximately 16 full-time jobs through construction. Additionally, there will be approximately 150 retail jobs created once operational.</p>
<p>What are the hours of operation?</p>	<p>The proposed hours of trade include:</p> <ul style="list-style-type: none"> Woolworths supermarket Monday to Sunday 6am to 12am (midnight) Specialty shops (general) Mondays to Sunday 9am to 6pm Loading dock Monday to Sunday 5am to 12am.
<p>What impacts will there be?</p>	<p>During construction there will be temporary disruptions to traffic and pedestrian access. There will be some noise and vibration associated with the work and mitigation measures will be used to reduce impacts. Updates will be provided to the community through construction phase.</p>
<p>How long will construction take?</p>	<p>If approved, construction would take approximately 16 months to complete. During this time, the project will provide up to 16 full-time equivalent construction jobs. The focus will be to provide opportunities for local businesses, builders and suppliers and use a regional workforce.</p>



addition to the supermarket.

Downloads

To download project factsheets, click on the buttons below.

Factsheet >

How to get involved

Attend the drop in session.

Take the survey

The community is invited to attend the **Drop-in session**.

When: Thursday 13 July 2023
Time: 3:00pm till 6:00pm
Where: Kingsway Plaza

We are currently seeking community feedback on the proposed development. To get involved:

- Take the **survey**
- **Submit feedback** by email
project@maraconsulting.com.au

About the consultation

Feedback gathered through the consultation will be included in a report as part of the Development Application (DA) for the proposed project and submitted to Cessnock City Council for assessment.

Mara Consulting is conducting the consultation on behalf of the developer. Kelly Lufberg is leading the project. She has more than 20 years' experience in communication and engagement.

To get in contact with the project team, email
project@maraconsulting.com.au

Need help? Think pink.

Got an idea or project you need to unpack?
We'd love to hear from you.
Call 0425 715 536.

First Name	Last Name
<input type="text"/>	<input type="text"/>
Phone	
<input type="text"/>	
Email *	
<input type="text"/>	
About	
<input type="text"/>	
Tell us about your project.	

Let's Chat!

“ **Creating vibrant communities through
powerful conversations**

